

An aerial photograph of a suburban neighborhood. The top half shows a dense residential area with many houses and trees. A road curves through the middle. The bottom half shows a commercial area with several large buildings, parking lots, and a multi-lane highway running horizontally across the frame. A bridge crosses a small waterway or drainage ditch near the highway.

# Urban Design Brief

1095 Kingston Road, Pickering  
April, 2025





# TABLE OF CONTENTS

## **01 INTRODUCTION 5**

INTENT 7

DOCUMENT STRUCTURE 7

## **02 PLANNING CONTEXT & SITE ANALYSIS 9**

STUDY AREA DESCRIPTION 10

STUDY AREA INVENTORY 13

PLANNING POLICY ENFORCEMENT 18

## **03 THE PROPOSAL 69**

DEVELOPMENT PROPOSAL 71

URBAN DESIGN VISION 88

## **CONCLUSION 101**

## **APPENDIX A 102**

SHADOW STUDY ANALYSIS 103

SHADOW STUDY SUMMARY 129





# 1 INTRODUCTION

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## 1.1. INTENT

The Biglieri Group Ltd. (“TBG”) has been retained by 1095 Kingston Road Ltd. (the “Owner”) to prepare an Urban Design Brief (“UDB”) in support of the proposed development for the lands municipally known as 1095 Kingston Road in the City of Pickering (the “Subject Site”). The proposed development consists of a four-tower residential development with a total of 1,492 units.

This Urban Design Brief outlines best practices for the design of the public, private and semi-private realm for the Subject Site within the context of the City of Pickering, and the Region of Durham. The goal of this document is to ensure excellence in urban design for the proposed residential development while addressing the character of the emerging neighbourhood and promoting increased density, walkability, and sustainability. The primary design principle is to provide a diversity of housing types that advances the existing and emerging built form.

The proposed development will accommodate a range of architectural elements consistent with the emerging neighbourhood identity and enhance the character and image of the established area, prioritizing pedestrian safety and sense of place, reinforcing a human-scaled built form, and promoting social interaction in the urban environment through architectural, urban design and landscape strategies. Through text supported by photographs, diagrams and illustrations, this document describes how the proposed site design and urban design forms demonstrate compliance with the Municipality’s guidelines as well as best Urban Design Practices.

## 1.2. DOCUMENT STRUCTURE

This document is organized into three sections:

- Section 1 - The Policy Context and Site Analysis (Policies, location and community context);
- Section 2 - Vision and Design Principles;
- Section 3 - The Design Concept that is responsive to the policy context and site analysis





# 2

## **POLICY CONTEXT & SITE ANALYSIS**



## 2.1. Study Area Description

### 2.1.1 Site Location

The subject site is located at the southwest corner of Kingston Road and Dixie Road and is municipally known as 1095 Kingston Road and is located in the Dunbarton/Liverpool Precinct. The site is legally described as:

*PT LT 25 CON 1 PICKERING PTS 1, 2 & 3, 40R1860 EXCEPT PT 1, 40R2670; S/T CO210581, S/T D486756, \*S/T D19631\* AS PARTIALLY RELEASED BY D314762; PICKERING. \*ADDED 2000 03 13 BY T.CUTLER*

The Subject Site is approximately 2.09 hectare (5.16 acres) and has approximately 255.6 metres of combined frontage on Kingston Road and secondary frontage along Dixie Road of approximately 172.3 metres. The Site currently has three (3) accesses from Dixie Road. The site currently hosts multiple commercial tenants in a one-storey structure. The site does not contain any Toronto and Region Conservation Authority's ("TRCA") Regulated Area. The southwest portion of the site abuts an active CN Rail Corridor.

The Subject Site topography is relatively flat, it slopes towards the southeast of the property with elevation ranges from approximately 91.1 metres above sea level (ASL) at the north corner to 85.5 metres ASL at the southeast corner.

Examination of the Subject Site larger context reveals the factors that will influence the proposed residential building and shape the proposed built form:

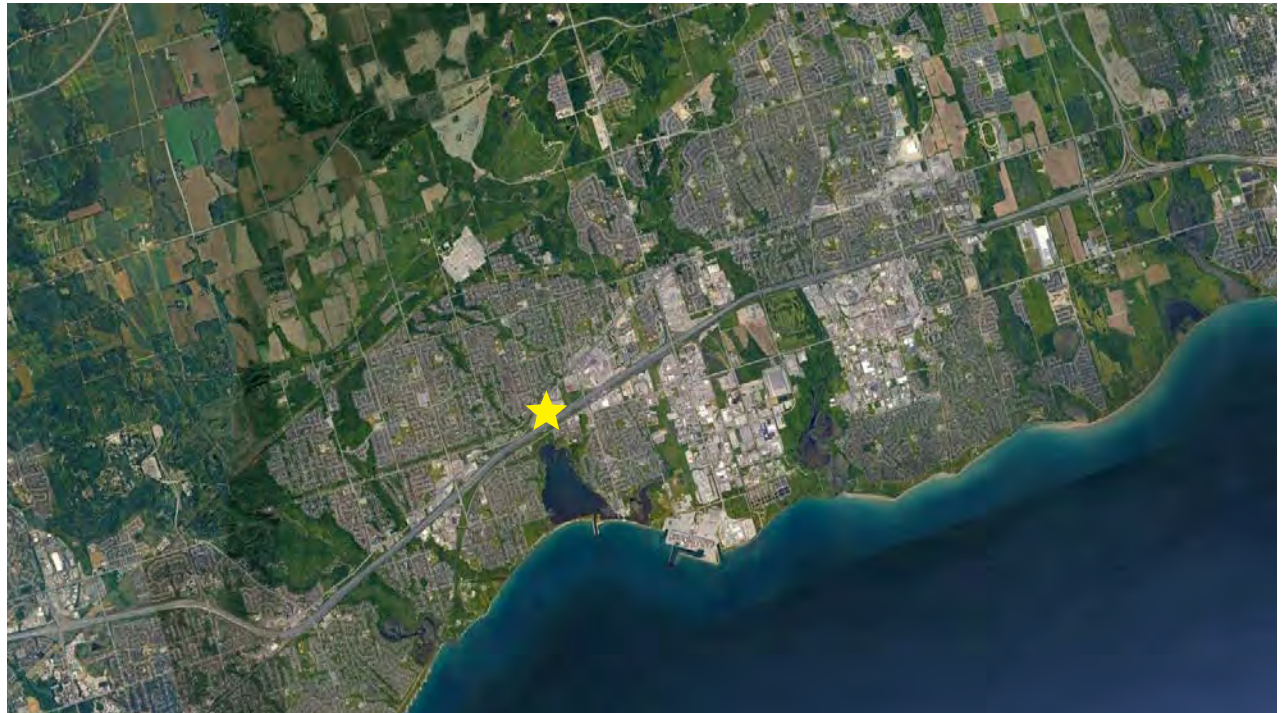


Figure 1. Site Location



### 2.1.2 Site Context

The Subject Site is located on the south side of Kingston Road, west of Dixie Road and north of Highway 401. The Subject Site is roughly triangular in shape and is surrounded by a mix of land uses. A detailed summary of the uses surrounding the Subject Site is included below:



Figure 2. Site Context





## NORTH

Kingston Road runs along the site's northern boundary; north of Kingston Road is an established residential neighbourhood consisting primarily of single detached dwellings.

## SOUTH

The southern portion of the site is bound by an active rail corridor and the Provincial Highway 401. Further south is Frenchman's Bay and other natural features.

## EAST

East of the Subject Site are large commercial plazas containing a range of retail uses include large and small format retailers. Planning applications for lands to the east are being considered by the City of Pickering. Given this evolving context along Kingston Road, it is expected that the lands to the east will eventually be redeveloped as mixed-use developments.

## WEST

Residential neighbourhoods consisting primarily of detached dwellings are to the west of the site and north of the rail corridor. Similar to the eastern context, commercial plazas along Kingston Road to the west of the site may be expected to evolve from a car oriented commercial form to a complete street with a range of built forms.



Figure 3. Surrounding Context: North and South (Google Earth)

Figure 4. Surrounding Context: East and West (Google Earth)

## 2.2. Study Area Inventory

### 2.2.1 Public Service Facilities

The Subject Site is within proximity of numerous public service facilities, such as schools, public transit, hospitals, sports facilities, and parks. Many of these facilities would be easily accessible to future residents of the proposed development. A breakdown of the public service facilities available to the Subject Site is detailed below:

#### Education:

The subject site is within the jurisdiction of the Durham District School Board (DDSB) and Durham Catholic District School Board (DCDSB).

The subject site is within the catchment areas for the following DDSB Schools:

- William Dunbar Public School (1.3 km)
- Maple Ridge Public School (2.2 km)
- Dunbarton High School (1.7 km)

The subject site is within the catchment areas for the following DCDSB Schools:

- Father Fenelon Catholic School (1.5 km)
- St. Isaac Jogues Catholic School (1.9 km)
- St. Mary Catholic Secondary School (2.1 km)

The future Durham-Scarborough-Durham BRT line will provide more convenient options to higher education facilities including University of Toronto Scarborough Campus and Durham College Oshawa Campus (approximately 14 and 15 km from the subject site)

#### Health and Wellness:

- Lakeridge Health Ajax Pickering is the nearest hospital located approximately 6.5 km from the subject site.
- Two pharmacies are located within walking distance of the site, Glendale Pharmacy and Shopper's Drug Mart are approximately 250 m and 400m away from the site, respectively.
- Various other health and wellness amenities are within 500 m of the subject site including a walk-in clinic, physiotherapy services and a family health team.

#### Parks and Recreation:

Frenchman's Bay is in proximity to the subject site, immediately to the south of Highway 401; this proximity will provide access to the many parks and recreational opportunities around the bay, including:

- Bruce Hanscombe Memorial Park
- Glen Ravine Park
- Douglas Park
- West Shore Beach
- Rotary Frenchman's Bay West Park

#### Community Services:

- The nearest Durham Region Police station, at 1710 Kingston Road, is located approximately 3.1 kilometres from the subject site.
- The nearest fire station is Pickering Fire Station 2, at 553 Kingston Road, approximately 1.8 kilometres from the subject site.
- Bank branches and ATMs for most major Canadian banks are within 1.5 km of the subject site.

### 2.2.2 Neighbourhood Built Form

The surrounding areas are comprised of a mix of land uses. The surrounding lands within the Kingston Road corridor are planned for redevelopment to mixed-use and higher density development. The residential areas to the north and south of the Subject Site are characterized by low-density single detached homes. The neighbourhood would benefit from higher densities that can be achieved through a wide range of residential typologies.

The Subject Site and surrounding area are identified as a mixed-use, rapid-transit corridor within the regional and local policy context. As such, the surrounding area is rapidly evolving to accommodate higher density mixed-use development that will more efficiently use land and municipal services. Higher density development is already evident in the immediate surrounding context of the Site, which includes a 12-storey mixed-use building and several townhouse developments.

### 2.2.3 Future Development

There are a variety of development applications and approvals within close proximity to the Subject Site (figure 6). These developments are in various stages of the planning process. A summary of these projects can be found below:

- 375 Kingston Road - 2-tower mixed-use proposal with 25 & 31 storey towers. Project is currently under appeal;
- 603-699 Kingston Road - 7-tower mixed-use proposal with towers ranging from 24 to 36 storeys. Project is currently under appeal;
- 705 Kingston Road – 5-tower mixed use proposal with towers of 28 to 35 storeys. Project is under review by the City.
- 875 Kingston Road - 2-tower mixed-use proposal with 17-storey towers. Project is currently under review;
- 1101-1163 Kingston Road - 14-tower mixed-use proposal with towers of 17 to 35 storeys. Project is currently under appeal.

A further analysis of these projects can be found in 2.5 of the Planning Rationale Report.



## 2.2.4 Transportation Network

### Road Network

- The Subject Site has frontage on the south side of Kingston Road, a Type B Arterial Road that extends between the City of Toronto east towards Pickering City Centre.
- Highway 401 is located immediately south of the Subject Site and is a provincial highway that provides connections west towards the City of Toronto and the GTHA, and east towards Oshawa and eastern Ontario.
- The nearest on/off ramps to the 401 are at Liverpool Road, approximately 1 kilometre east of the Subject Site.

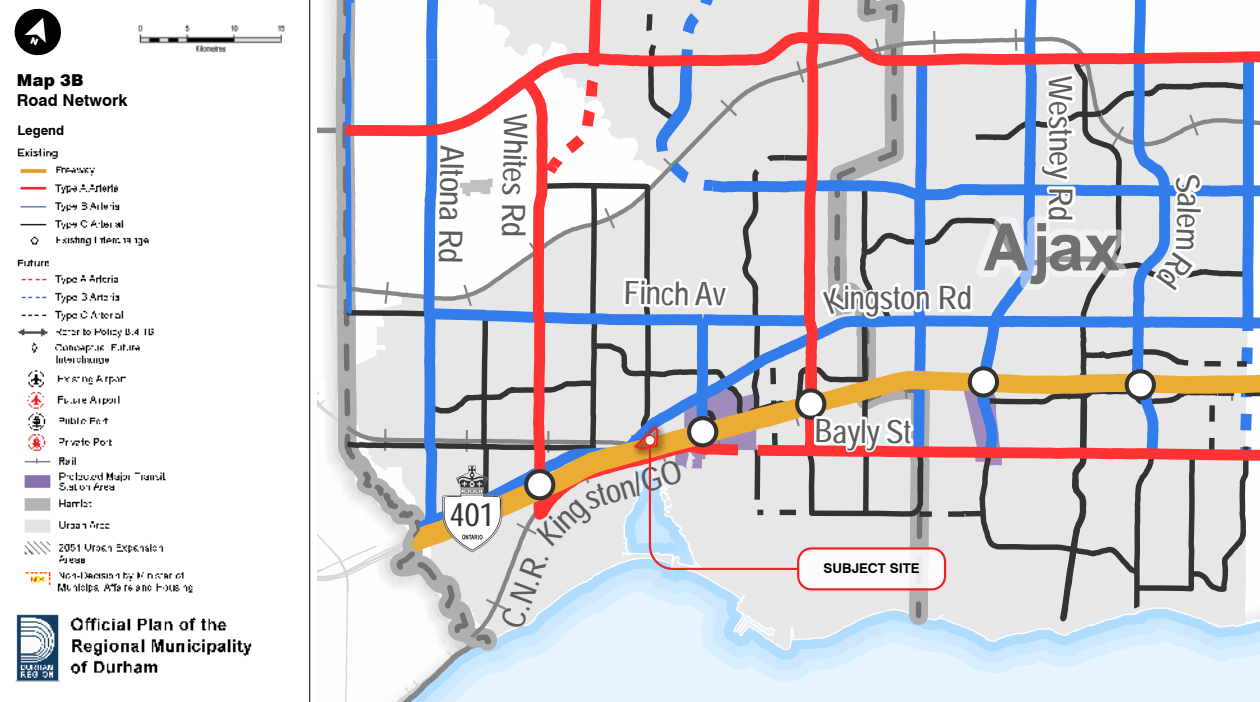


Figure 6. Durham Official Plan: Map 3B. Road Network

## Public Transportation Network

- The Subject Site is serviced by Durham Region Transit (DRT) and GO Transit.
- Two stops are located adjacent to the Subject Site at the intersection of Kingston Road and Dixie Road. The stops are served by three DRT routes:
  - Route 118: North-South connections along Kingston Road and Whites Road to Tauton Road.
  - Route 900: Rapid bus route providing east-west connectivity from Centennial College's Morningside Campus in Toronto to Downtown Oshawa; lettered branches serve other locations in Oshawa.
  - Route 920: Rapid bus route providing regional connections from Scarborough Center in Toronto and Harmony Terminal in Oshawa.
- The Subject Site is located along the future Durham-Scarborough Bus Rapid Transit ("BRT") route which will provide rapid-bus transit along Kingston Road from Scarborough Centre to Downtown Oshawa. A future station stop is expected to be at Dixie Road, in close proximity to the Subject Site.

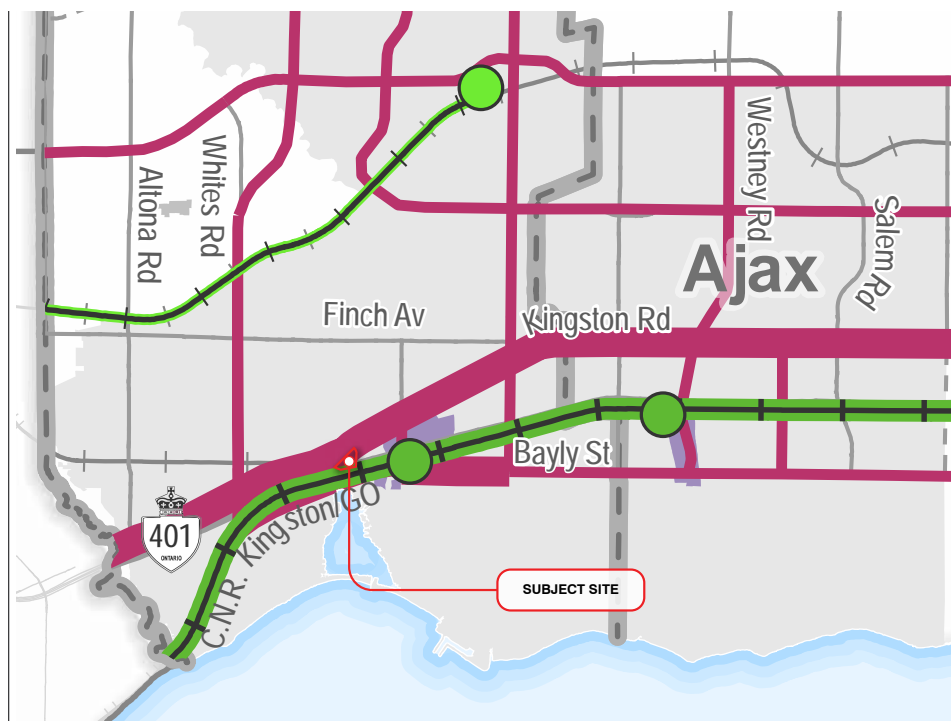


Figure 7. Durham Official Plan: Map 3a. Transit Priority Network

## Active Transportation Network

- The existing condition of Kingston Road represents a gap in the active transportation network, as no sidewalks exist along the south side of Kingston Road. Sidewalks exist along Dixie Road running south of Kingston Road.
- Improvements to Kingston Road are proposed in conjunction with the Durham-Scarborough BRT project. Upon full build out, dedicated cycling infrastructure and pedestrian facilities are planned along both sides of Kingston Road.

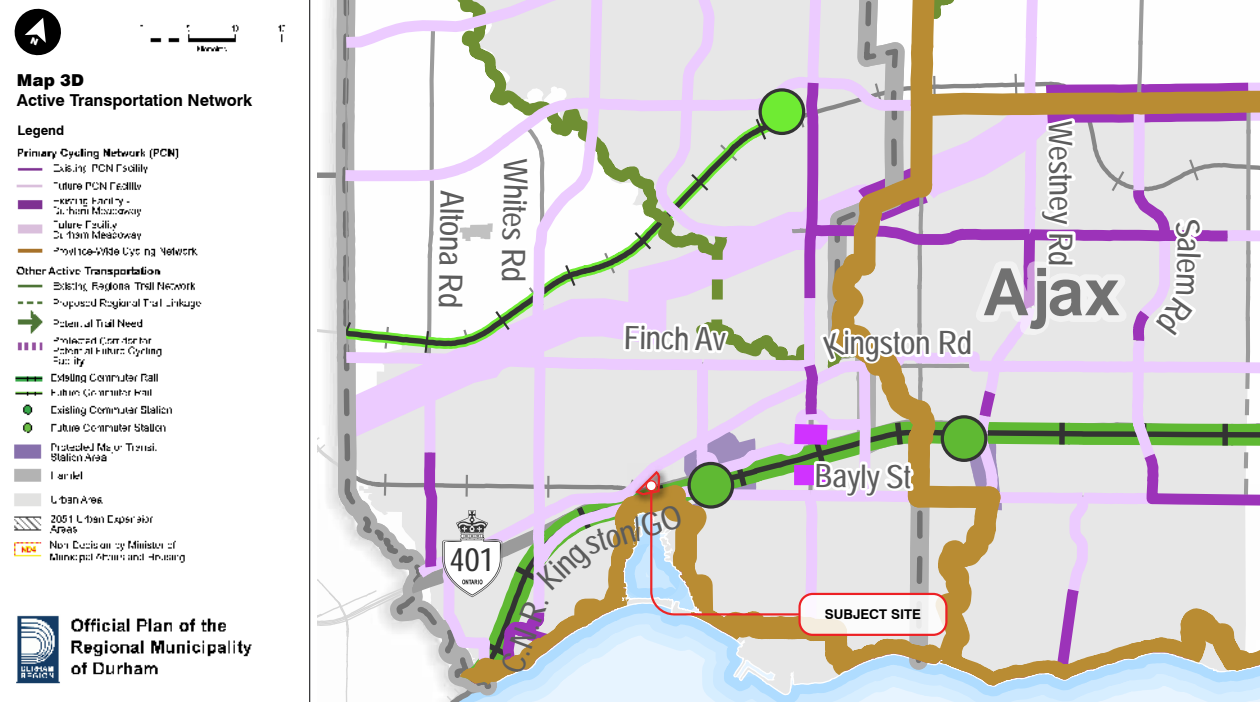


Figure 8. Durham Official Plan: Map 3d. Active Transportation Network

## 2.3. Planning Policy Enforcement

This Urban Design Brief provides an overview of the in-effect planning policy framework that guides development in the City of Pickering. This includes provincial, regional and municipal policy documents, as well as accompanying design guidelines. This section highlights applicable policies and discusses how the proposed development responds to each applicable policy section.

This section will detail how the proposed development engages with policies from the following documents:

- Provincial Planning Statement (2024);
- The Durham Regional Official Plan (May 2024 Consolidation);
- The Municipality Of Pickering Official Plan (2022);
- Municipality of Pickering Zoning By-Law No. 3036;
- Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines (2019); and
- Kingston Road Corridor and Specialty Retailing Node Intensification Plan (2019).

## 2.3.1 Provincial Planning Statement (2024) (“PPS”)

### Policy

2.1.6. Planning authorities should support the achievement of complete communities by:

- (a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, longterm care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;
- (b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and
- (c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.

2.2.1 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by permitting and facilitating intensification of underutilized commercial sites, promoting densities with efficiently use resources, and requiring transit-supporting development in proximity to transit corridors.

2.3.1.3 Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.

### Response

The proposed residential development is consistent with the PPS’s intensification and mixed land use policies. The proposed residential development will:

- Accommodate an appropriate range and mix of residential units;
- Aid in increasing the current mix of residential uses in the area surrounding the Subject Site to promote efficient development and land use patterns;
- Promote intensification in the municipality on an existing under-utilized commercial site, and will optimize the use of existing infrastructure and transit investments;
- Minimize land consumption and climate change impacts;
- Provide for an appropriate mix and range of residential typologies to meet long-term needs of future and existing residents; and,
- Encourage more compact residential uses to allow for the efficient use of land, promote growth and development, and avoids risks to public health and safety.

## 2.3.2. The Durham Regional Official Plan, Envision Durham (2024)

Policy	Response
<p>The Envision Durham Adopted Official Plan, as approved (2024) is a guiding document to inform growth and development within Durham Region. Development in the Region must conform with the policies of the Regional Official Plan.</p> <p>The Subject Site is designated as Rapid Transit Corridor (Figure 6) on Map 1 - Regional Structure-Urban &amp; Rural Systems. Per the Regional Official Plan, Rapid Transit Corridors are intended to provide essential connections to other Strategic Grown Areas in the region and have been assigned a transit supportive density to support the implementation of planned higher order transit service.</p> <p>A minimum transit supportive density of 150 people and jobs per hectare has been identified for Rapid Transit Corridors.</p> <p>It is the policy of the Council to:</p> <p>5.2.8 Incorporate transit-oriented development design principles such as:</p> <p>ii) Orient development and entrances towards streets and towards transit station and stop locations;</p>	<p>iii) Incorporate design elements that contribute to complete, active and pedestrian-oriented streets and public places as part of a high-quality public realm;</p> <p>iv) Provide active uses and entrances at grade, and integrate open space, parks and plazas along with public art and community spaces.</p> <p>vi) Provide compact built form with densities ranging from medium to high-density, with the highest densities located closest to transit stations.</p> <p>5.2.24 Support the planning and development of a built form that is compact, pedestrian-friendly, and implements transit-oriented development along Rapid Transit Corridors. A full range of mixed uses including commercial, retail, institutional, residential, personal services, offices and other uses are to be provided for.</p> <p>5.2.26 b) Permit a full range and mix of uses including residential, commercial, compatible employment uses such as offices, and other uses, in a higher density, compact and pedestrian-oriented built form.</p> <p>The proposed development provides high density residential dwellings that will add to the mix of housing types that is predominately single-detached. The built form is compact, efficient, accessible, and comprised of a mix of land uses. The proposed development includes 1,492 residential units and offers a range of unit typologies including 1 Bedroom, 1 Bedroom+Den, 2 Bedrooms, 2 Bedrooms+Den and 3 Bedrooms.</p> <p>The compact built from and location along a transit route encourages alternative forms of transportation other than the private automobile. The proposed development includes a total of 895 bicycle parking spaces and 1,143 vehicular parking spaces.</p>

Residential lobbies and interior amenities which interface with the public realm will help activate the ground floor and contribute to a pedestrian oriented streetscape along both Kingston and Dixie Roads. A wide-boulevard-style landscaping treatment along Kingston Road will also enhance the at-grade experience. The proposed POPS, Gateway Plaza, and Multi-use Path will improve the pedestrian and cyclist experience. A mid-block connection between the Kingston Road and the POPS to the south, will further bolster the streetscape and support connectivity.



Figure 9. Durham Official Plan: Map 1. Regional Structure



### 2.3.3. The Municipality Of Pickering Official Plan (2022)

Policy

The Municipality of Pickering Official Plan provides a framework for future growth and development. The Official Plan outlines a vision for building a good community and is intended to guide local growth and development through the designation of land for various land uses and providing policies that will guide the development and use of land city-wide.

This section analyzes all relevant Official Plan goals and policies which apply to the proposed development.

### Chapter 3 Land Use

Chapter 3 of the Pickering Official Plan contains strategies for 12 primary land use categories. The proposed development is located within Mixed Use Areas. Schedule I to the Pickering Official Plan identifies the subject site as a Mixed Corridor. This section also speaks to the permitted uses and densities in Mixed Use areas. It should be noted that certain sites may qualify for additional density, should they meet specific criteria. See the Planning Rational Report for a comprehensive discussion of how the proposal and subject site meet the additional density criteria.

#### MIXED USE AREAS

3.6 To achieve the community design goal, City Council shall:

- (a) Recognize as Mixed Use Areas on Schedule I, lands that have or are intended to have the widest variety of uses and highest levels of activities in the City;
- (b) Shall ensure Mixed Use Areas are designed and developed consistent with the community design provisions of this Plan (Chapters 9 and 14), and any development guidelines that may be established in a Part 3 Neighbourhood Plan (Chapter 12);

Chapter 11A of the Official Plan provides further policy guidance for Mixed Use Areas along the Kingston Road Corridor. The subject site has been identified as Mixed Use Type C which are to be developed predominantly as residential areas with limited retail uses. See the Planning Rationale Report for a fulsome analysis of Chapter 11A policies against the proposed development.

#### Response

In alignment with Mixed Use policies that apply to the subject site, the proposal prioritizes residential uses. Lands to the east of the subject site are planned to retain their commercial character as they are redeveloped and will continue to meet the retail and service needs of future residents.

## Chapter 6 Housing

### Policy

The housing strategy aims to promote opportunities for a wide variety of housing forms, tenure and types to meet the evolving needs of Pickering's residents. The following housing policies and goals are as follows:

### HOUSING GOAL

6.1 Encourage a broad range of housing by form, size and location, with specific targets for new apartment construction in the urban area.

### HOUSING OBJECTIVES

6.2 City Council shall:

- (a) encourage housing opportunities that respond to the existing and future needs and characteristics of the population;
- (b) encourage the provision of an adequate range of housing and tenure types; and
- (c) encourage the provision of an adequate supply of housing throughout the City in terms of quantity, quality and diversity, including the provision of an adequate supply of affordable, rental, assisted and special needs housing.

### HOUSING MIX AND SUPPLY

6.3 Promote an adequate supply and mix of housing by:

- (a) encouraging the production of new residential dwelling units in accordance with housing targets for average annual production, unit mix, and location.

### AFFORDABLE AND SPECIAL NEEDS HOUSING

6.4 Require that a minimum 25 percent of new residential construction, on a City-wide basis, be of forms that would be affordable to households of low or moderate income.

### INFILL, INTENSIFICATION AND REDEVELOPMENT

6.5 Maximize the efficiency of existing infrastructure and minimize the consumption of vacant land by encouraging major intensification in Mixed-Use Areas and infill development on under-utilized blocks of land.

Response

The proposed development provides a range of unit sizes and types to meet the diverse needs of residents. This approach will serve to diversify the existing housing stock in the community, which largely consists of single-family homes and provide housing options for different communities. The proposed density of the development represents the significant intensification of an under-utilized site designated as Mixed-Use Areas. The tenure of the proposed units will be determined at a later date. No affordable units are currently included as part of the proposal. This significant contribution to the housing supply in the area may impact supply and subsequently housing prices in the area.

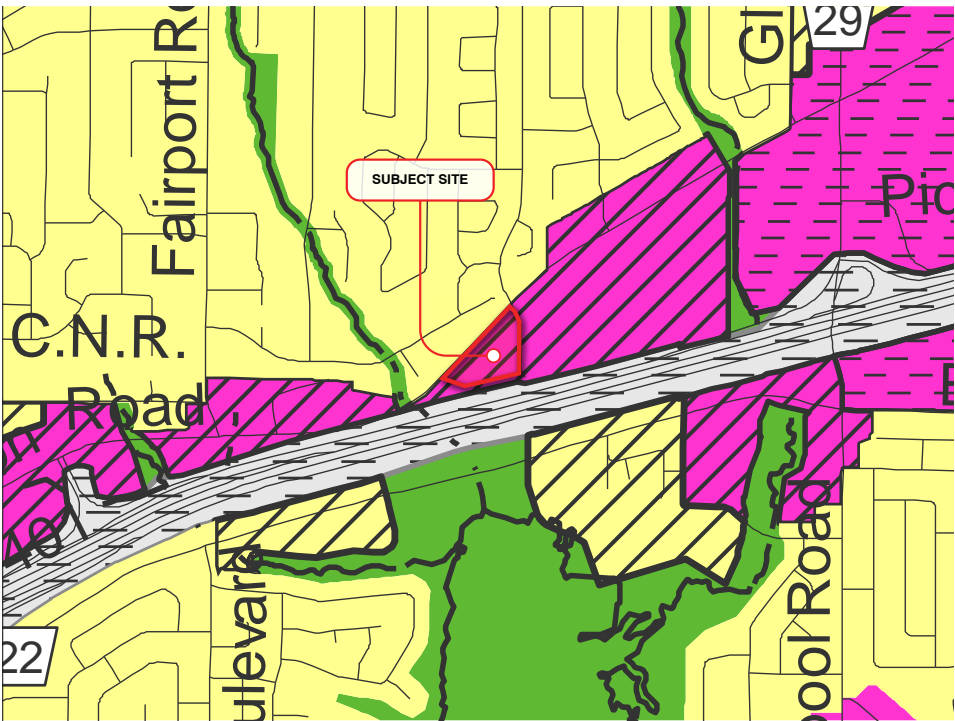
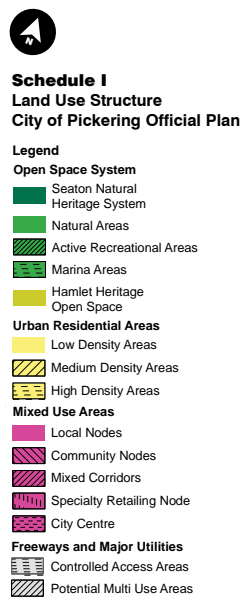


Figure 10. Pickering Official Plan: Schedule 1. Land Use Structure

## Chapter 9 Community Design

### Policy

Chapter 9 - Community Design of The Municipality of Pickering Official Plan sets outlines a series of policies to guide community design. This discussed the ten community design concerns focusing on the creation of high quality built environments. Pickering's Ten Community Design Concerns are as follows:

1. Human Scale
2. Pedestrian Comfort
3. Mixed Uses
4. Permeability
5. Context
6. Building Adaptability
7. Places Versus Buildings
8. Attractive Public Spaces
9. Legibility
10. Natural Heritage.

The following policies relate to the ten design concerns identified above.

### COMMUNITY DESIGN GOAL

9.1 City Council shall promote developments at various scales which, through their adherence to principles of good, high quality community design, will produce built and natural environments in Pickering that offer enjoyment, comfort and safety for all users, and evoke a desirable image and sense of place for the City.

### COMMUNITY DESIGN OBJECTIVES

9.2 To achieve the community design goal, City Council shall:

- (a) Encourage the creation of an overall physical form for Pickering that is related to the scale and pace of pedestrians;
- (b) Encourage private and public developments that offer pedestrians and users a high level of comfort, enjoyment and personal protection;
- (c) Encourage private and public developments that provide an integrated mix of uses, activities and experiences;
- (d) Encourage the design of road patterns, buildings and the spaces between them in a manner that supports an efficient public transit system and makes it easy for both pedestrians and vehicles to move about in a variety of directions;

(e) Encourage developments that are designed to fit their contexts by considering the mix of uses, and the massing, height, scale, architectural style and details of existing, adjacent buildings;

(f) Encourage developments that create spaces between and along buildings that are of high architectural and landscape quality, and contribute to and enhance the overall quality of Pickering's public realm;

(g) Encourage, where appropriate, the creation of landmarks and other distinctive elements including buildings, open spaces, landscapes and natural features that make it easy for people to understand where they are, and how they get to the various places, amenities and facilities they require;

## Response

(h) Encourage the design of buildings and places that can be used for a variety of purposes, and are capable of adapting over time to changing circumstances and opportunities;

(i) Encourage the use of colour, decoration and variation in material to create buildings, and the spaces around buildings, that are attractive for people to look at and use; and

(j) Encourage developments that establish appropriate relationships between built and natural environments, that ensure sensitive natural systems are protected and where possible enhanced, and celebrate significant aspects of the natural and cultural landscape.

The proposed development aligns with the goals and objectives outlined in Chapter 9 by creating a well designed, compact, residential development within the urban area of the City of Pickering.

The Kingston Road corridor is an area in transition. Originally occupied largely by commercial plazas and associated parking; the evolving policy context has allowed mixed-use, higher intensity projects along this spine. The proposed development represents a high-quality addition to this evolving development context. The pedestrianized character along Kingston Road, with the proposed 6-storey streetwall, will support the vibrancy and walkability of the corridor as the community continues to mature. Furthermore, the planned residential lobby entrances and amenity space will animate the streetscape.

Central to the proposal's design is the network of pathways way that frame the development along Kingston Road and Dixie Road to create an animated pedestrian experience. Pedestrian pathways through the centre of the site will also provide additional connectivity through to the proposed POPS and Multi-use Path. The

proposed driveways accesses along Kingston Road and Dixie Road will minimizes pedestrian risk by concentrating vehicular uses to the west and south of the site, mostly hidden from view, and largely away from pedestrian circulation. The proposed open space network consisting of the Gateway Plaza, POPS and Multi-use Path, are expected to be of high design quality and will support connectivity throughout the community, while offering signature spaces for residents to rest and socialize.

Materiality and architectural expression may be refined and modified over the course of future resubmissions and through the Site Plan Approval process. However, the project team expects the built form to be developed using quality materials, with emphasis on colour, decoration and variation, where possible. Materiality on podium levels containing above-grade parking is expected to mimic that of residential uses. As such, there will be minimal visual impact from above grade parking on road users.

## Chapter 12 Urban Neighbourhoods: Liverpool Neighbourhood

### Policy

Chapter 12 - Urban Neighbourhoods of The Municipality of Pickering Official Plan is supplementary to the land use information included in Chapter 3. This Chapter provides detailed neighbourhood policies for each of the City's urban neighbourhoods.

The Subject Site is situated in Neighbourhood 12: Liverpool (figure 11).

- This chapter of the Official Plan outlines the following objectives:
- The subject site is within the lands for which the City is expected to adopted development guidelines
- Requires new development on the subject site to be in accordance with Chapter 11A of the Official Plan
- Promotes reduction of traffic speeds along Dixie Road and the improvement of pedestrian safety

### Response

At the time of writing, no compendium document regarding development guidelines has been made available for the Liverpool neighbourhood. As demonstrated in the Planning Rationale Report, the proposal meets the general intent of Chapter 11A. Later sections of this report will demonstrate how the proposal is aligned with the Kingston Road Corridor Urban Design Guidelines. In an effort to support pedestrian safety through the site, the proposal integrates wide walkways along both Kingston and Dixie Roads, as well as walkways through the centre of the site. Additionally, curb cuts for vehicular traffic have been minimized and strategically located to minimize conflict between pedestrians and vehicles.



**Map 22**  
**Neighbourhood 12: Liverpool**  
**City of Pickering Official Plan**

**Legend**

- New Road Connections (Proposed)
- - - Detailed Review Area
- Kingston Road Corridor Development Guidelines (Refer to Compendium Document)
- Lands for which Council has adopted Development Guidelines (Refer to Compendium Document)
- ↔ Pedestrian/Bicycle Connection (Existing)
- ↔ Pedestrian/Bicycle Connection (Proposed)
- Neighbourhood Boundary
- Waterfront Trail
- Regional Flood Line
- Place Of Worship
- Public Elementary School
- Separate Elementary School
- Public Secondary School
- Fire Station
- Park

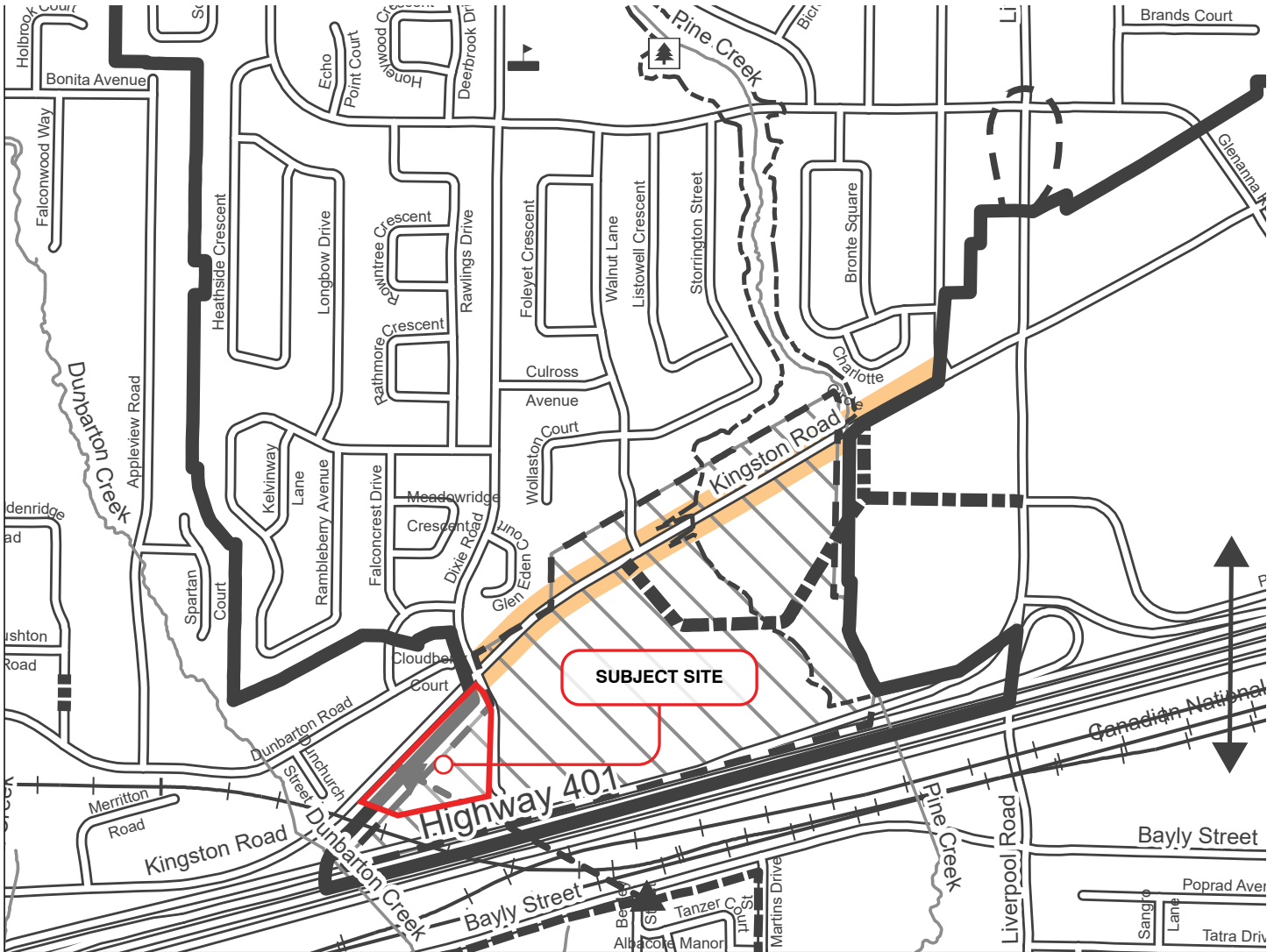


Figure 11. Pickering Official Plan - Map 22: Liverpool Neighbourhood



## Chapter 14 Detailed Design Considerations

Chapter 14 - Detailed Design Considerations of The Municipality of Pickering Official Plan is supplementary to the land use information included in Chapter 9. The Detailed Design Considerations are as follows:

1. Community Image
2. Corporate Initiatives
3. Design with Nature
4. Development and Subdivision Design
5. Views and Vistas
6. Design of Public Open Spaces
7. Streetscapes
8. Human Scale
9. Design of Buildings
10. Personal Security
11. Barrier-Free Access
12. Public Art
13. Lighting
14. Signage
15. Pedestrian and Cycle Safety

As indicated in Chapter 14, City Council shall identify and establish appropriate community design policies for the Detailed Design Considerations. The Policies related to the relevant Detailed Design Considerations are as follows:

### COMMUNITY IMAGE

#### Policy

14.2 Community Council shall:

- (a) require that development at all scales creates, reinforces, and enhances distinctive neighbourhoods, nodes and corridors, and enhances the specific character of existing developments and neighbourhoods; and
- (b) consider identifying at certain locations in the City, gateways and landmarks and require that these locations be maintained and enhanced through community design measures.

#### Response

The proposed development includes active residential lobby uses and indoor amenity space at grade along Kingston Road contributing to establishing Kingston Road as a “mainstreet” for Pickering. Planters, trees and boulevard paving contribute to the mainstreet-like character along Kingston Road. Buildings are appropriately located close to the property line creating a strong streetwall along the south side of Kingston Road. The proposed Gateway Plaza serves to create a signature gateway space on the subject site. The Gateway Plaza is strategically placed in proximity to the future BRT stop, reinforcing the corner of Kingston Road and Dixie Road as a gateway into the Liverpool Neighborhood and the Dunbarton Liverpool Precinct.

DESIGN WITH NATURE

Policy

Good community design should, where possible integrate and connect to Pickering’s natural features and resources.

14.4 Community Council shall:

- (b) where possible, require the maximum retention of natural features on the properties proposed to be developed;
- (e) encourage the use of plant materials in a design capacity to define open spaces, frame desired views or focal points, direct pedestrian movement, and reinforce particular locations;
- (f) encourage the use of plant materials to create visual variety on the basis of their form, colour and texture, and to satisfy functional requirements, such as providing shade, providing screening in all seasons, providing sound attenuation, buffering wind, controlling snow deposition, and stabilizing slopes;

- (g) in certain areas of the City, encourage the use of low maintenance plant and landscape materials which enhance ecological stability; and
- (h) encourage the use of native plant species which are tolerant to disease and pollutants as the dominant plant material when creating new plant communities or when adding to existing plant communities.

Response

The proposed development introduces planting design throughout the subject site to emphasize pedestrian zones, places of gathering, building entrances and to embellish the pedestrian environment. The existing subject site has minimal existing natural features, as it is primarily paved over. Landscape plantings have been curated based on soil volumes above the below grade parking. As much as possible, the project team has worked to integrate trees into the landscape plans in key areas such as along Kingston Road, the Gateway Plaza, the POPS and the mid-block connection. Furthermore, the 14.0 metre MTO setback will be retained for exterior landscape with a buffer approach to planting. Landscape plans will continue being refined, but are expected to leverage native vegetation where possible.

## DEVELOPMENT & SUBDIVISION DESIGN

### Policy

14.5 Community Council shall:

- (a) encourage designs and patterns for streets and major aisles that provide appropriate access for vehicles, public transit, pedestrians and cyclists; create view corridors and vistas where appropriate; and allow adequate space for utilities and services;
- (b) encourage designs of streets, major aisles, blocks and lots that create a public realm supporting comfortable and safe pedestrian activity and movement both within and beyond the development;
- (i) require the implementation of green development standards in development and subdivision design, in keeping with applicable City policies and guidelines

### Response

The development proposes no new public roads. Appropriate access to vehicles is being provide via three private drive aisles, one along Kingston Road and the other along Dixie Road. The minimization of curb cuts will minimize conflicts between road users, providing comfortable experiences for pedestrians and cyclists. Planters, landscaping and patterned pavers will also support a pleasant pedestrian experience both within and on the periphery of the proposed development. Green standards have been integrated where possible and includes contributions to the urban tree canopy and on-site stormwater retention.

## VIEWS AND VISTAS

### Policy

14.6 Community Council shall:

- (a) recognize significant views of prominent buildings and open spaces at the scales of neighbourhoods, streets, small public spaces and individual development sites;
- (b) preserve “landmark” views of unique features, including the Pickering Civic Complex and Frenchman’s Bay, to provide visual reference points within Pickering’s urban area, and to enhance the significance of those features;
- (c) evaluate new development proposals for their opportunity to maximize, create or enhance views and vistas;
- (d) endeavour to maintain and enhance views of natural features, including woodlots, topographic features, bodies of water and across open spaces;
- (f) endeavour to ensure that the design of sidewalks and other portions of buildings adjacent to public spaces provides views from exterior to interior activity areas, including stairwells, corridors, and entrance and elevator lobbies.

### Response

Given the location of the subject site, the proposed development will be highly visible from Highway 401. Consideration has been taken regarding the allocation of tower height, architectural design, and building massing to enhance the views of the proposal. The location of residential entrances have been situated to ensure they are highly visible from Kingston Road. Interfaces between interior amenity spaces and the Kingston Road frontages further improve the ‘eyes on the street’ of the public realm.

The subject site is north of Frenchman’s Bay. However, due to the active rail corridor and Highway 401, visibility of this natural feature from the subject site is mostly limited. The proposed development provides views onto Frenchman’s Bay by locating residential space above the podium parking levels. In this way, the development uses design strategies to provide residential views onto significant vistas and bodies of water, as directed by the guideline.

## DESIGN OF PUBLIC OPEN SPACES

### Policy

Public open spaces should be fully integrated with, and supported by, the developments around them.

14.7 Community Council shall:

- (a) promote the design, preservation, enhancement and creation of significant public open spaces in both the urban and rural areas that contribute to the City's image;
- (b) encourage public open spaces that complement and support the uses and activities generated by surrounding buildings and uses;
- (d) encourage in urban areas the creation of smaller outdoor spaces such as small parks, gardens and courtyards, where appropriate, and endeavour to ensure these spaces are defined and complemented by the architectural and design features and the scale of the buildings that surround them;
- (e) encourage within publicly-accessible open spaces, a high quality environment with adequate amenities such as appropriate paving, benches, bicycle racks, refuse containers, lighting and other elements that accommodate the intended users of the space;

- (g) encourage the design of open spaces to consider the user's sensory experiences of light, sound, smell, colour, water and temperature; and
- (h) encourage the design of private space adjacent to public streets and open space areas (e.g., outdoor patios) to support the function and enhance the appearance of the adjacent public streets or areas.

### Response

The proposal includes multiple publicly accessible open spaces: the Multi-use Path, the Gateway Plaza and the POPS. These spaces are expected to provide a range of benefits to future residents include passive and active recreation, rest and socialization. The Gateway Plaza and POPS will include a range of seating and planting options as well as public art. The POPS is expected to include a dog run, a playground and a water feature, welcoming a wide range of users and visitors. These spaces will be multi-sensory by means of their integration of public art and water features. Private spaces which interface with the public realm, such as the interior amenities facing Kingston Road or the loading area facing the Gateway Plaza, are expected to be designed with a high regard for materiality, form and function.

## STREETSCAPES

### Policy

14.8 Community Council shall:

(a) support the creation of specialty treatments including planted boulevards and median strips, theme lighting and street furniture, and other design features, on strategic streets in Pickering;

(b) encourage landscape design along streets to complement adjacent built forms and open spaces, to provide shade in the summer and visual interest throughout all seasons, and to accentuate the special character of particular streets;

(c) support, where appropriate, the use of sidewalks and adjacent publicly-accessible open spaces as outdoor patio restaurants;

(d) promote a unified design of decorative treatment for sidewalks within strategic areas, such as the City Centre, community nodes and other important shopping areas;

(h) endeavour to ensure that seating on public and private streets is provided for pedestrians at waiting areas, bus stops, and near public facilities and institutions, and to support leisure activities, conversation and social interaction in commercial, civic or mixed-use areas;

(i) require the provision of secure bicycle parking facilities on public streets, at bus terminals, transit stations, GO stations and near entrances to buildings that are important destinations, such as retail commercial buildings, and community or cultural facilities; and

(j) endeavour to ensure that the design and pattern of pavement for pedestrian paths and sidewalks enhance the character of high activity areas along the street; indicate pedestrian crossing with a continuation of the sidewalk pattern over the crosswalk; indicate points where vehicular routes cross pedestrian paths; and accommodate higher volumes of pedestrian movement by widening sidewalks at intersections;

(k) require the planning and design of roads such that the placement of underground utilities supports the planting of trees and other large scale plant materials;

(o) encourage an underground location for local utility lines and cables;

(q) support, where appropriate, the provision of cycling lanes within the paved surface of roads, which are separated from vehicular traffic by design features such as distinctive surface treatments, painted lines, symbols and signage;

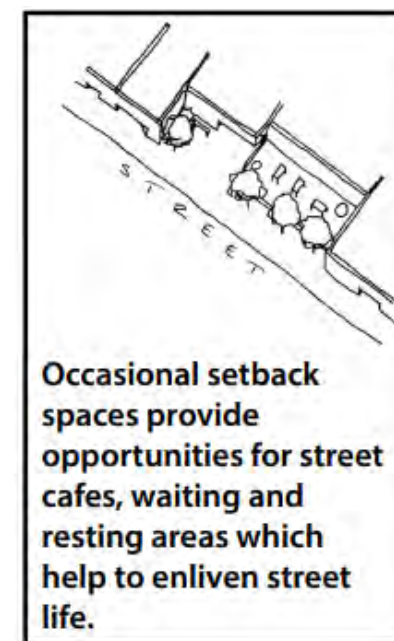


Figure 12. Pickering Official Plan - Setback Spaces



## Response

The proposal includes improvements to the Kingston Road streetscape condition. In order to improve the road experience, the buildings have been setback from the property line, tree planters, seating have been integrated and arcade unit pavers have been integrated. Patterned coloured paving will be deployed in strategic areas such as the Gateway Plaza, POPS and mid-block connection to create visual connections towards points of interest. The Gateway POPS will include seating opportunities and plantings in proximity to the future BRT stop. Utilities have largely been coordinated below grade within the underground parking level.

Underground parking and servicing have been designed to maximize soil depths, allowing for trees to be planted in strategic locations on the subject site. Secure and visible bicycle parking will be provided on the site, in proximity to the future BRT stop. The proposal is supportive of the ultimate road condition for Kingston Road which is expected to include separated cycle lanes.

## HUMAN SCALE

### Policy

14.9 Community Council shall:

- (a) encourage the use of continuous horizontal projections such as cornices, roof overhangs or masonry courses within the first few storeys of buildings adjacent to pedestrian routes to establish human scaled visual and physical references;
- (b) encourage development at heights that are related to the width of the streets they front in order to establish a sense of enclosure along the public sidewalk, and to ensure reasonable sunlight on the street;
- (c) encourage building designs that capitalize on the use of grade level windows and doors to permit visibility of human activities within the public areas of buildings;
- (d) encourage the use of trees and shrubs in areas of more intense development or within large open spaces to create human scale; and
- (e) promote the design of buildings, spaces, and facilities to accommodate the varied range of human dimensions, levels of mobility and strengths.

### Response

The proposed 6-storey heights of the podiums along the public realm are of a human scale and contain a high level of glazing and active uses at grade. The recessed entrances will provide an animated and pedestrian oriented environment along Kingston Road. Trees will be planted along streetscapes where possible to reinforce the human scale long Kingston and Dixie Roads. Kingston Road is identified as a Type B Arterial road by the Regional municipality of Durham. The podium heights will reach 33m indicating that the podium is informed by the future right-of-way width of 45 metres and will allow for sunlight to penetrate to grade level.



Figure 13. Pickering Official Plan - Human Scale

## DESIGN OF BUILDINGS

### Policy

A high level of architectural standards is important in design and construction of Pickering's built environment. This will be achieved by:

14.10 Community Council shall:

- (a) encourage buildings that can be identified and appreciated at various scales, including up close, from the immediate area (including nearby streets that offer direct views of the building), and when appropriate, from locations beyond the immediate area;
- (b) where groupings of buildings are proposed, require built forms, massing and architectural treatments that create cohesive and unified developments, and are architecturally compatible with each other and surrounding areas;
- (d) require designs that present continuous building façades along major streets and express design elements such as floor and ceiling levels, window heights, columns and internal divisions, to assist in defining human scale and providing visual interest;
- (e) discourage the placement of building functions which do not directly support public activities, such as loading bays, utility rooms and other building mechanical features (e.g., exhaust grilles), from being located on building façades adjacent to streets;

- (f) require the orientation of the main front entrances to commercial, industrial, apartment and public buildings towards the street whenever possible, and to be visible from main pedestrian routes and vehicular approaches;
- (h) require the height, form, massing and articulation of the façade of new buildings to reflect its "position" or significance on the street;
- (i) endeavour to ensure that building designs provide opportunity for protection from the elements (rain, snow, wind and sun) through the use of features such as awnings, canopies, colonnades or recessed ground floor façades;
- (j) require the incorporation of bicycle storage areas in high density residential, commercial and major industrial buildings and sites;
- (k) encourage the use of high quality, low maintenance building materials to help ensure an attractive appearance over time;
- (l) discourage the use of corporate image building design and promote design which reflects neighbourhood character; and
- (o) encourage the implementation of green development standards in the design of buildings.

### Response

The proposed development uses a variety of building massing and scale to ensure a cohesive and unified development is achieved. Buildings have been situated and massed to ensure a meaningful public realm is carved out of the site. Residential entrances are orientated towards the street and along pedestrian routes. Loading and servicing uses are concentrated away from the public realm and shielded from view.

Tower height and massing is distributed across the site providing a sculpted silhouette for the proposed development. The long linear tower floorplates are mostly skewed in a north-south direction and will thus cast narrow, fast moving shadows on the properties to the north.

Green development standards will be deployed where possible including: on-site stormwater management tanks, green roofs, and improved landscaping.

## PERSONAL SECURITY

### Policy

A high level of personal security is an important objective of community design. The following policies relate to Personal Security:

14.11 Community Council shall:

- (a) endeavour to ensure that the design of developments minimize conditions that are potentially dangerous to the public without impeding functional and aesthetic characteristics;
- (b) encourage the continuous occupancy and use of public spaces throughout daily, weekly and seasonal cycles by encouraging the mixing of spaces, activities and institutions which enable public presence at varied times;
- (c) discourage developments from having public and publicly-accessible spaces such as parking facilities, outdoor and indoor walkways, elevators, stairs and lobbies in remote or isolated locations;
- (d) endeavour to ensure publicly-accessible spaces are located near public roads, transit stops and other high activity spaces to enable public surveillance;

- (e) endeavour to ensure landscaping plants and materials are used in a manner that does not obstruct views into lobbies, windows, parking facilities and pathways, or any other views needed to ensure clear surveillance and safety;
- (f) endeavour to ensure views are provided into, out-of, and through publicly-accessible interior spaces of developments through the use of transparent materials in stairways, lobbies, hallways, elevators and doors;
- (g) discourage the creation of long passages or outdoor walks which cannot be adequately watched or monitored;
- (h) endeavour to ensure adequate lighting, early detection (e.g., mirrors and transparency), and remote monitoring (e.g., cameras) are used in locations where personal security risks may be present; and
- (i) endeavour to ensure developments are designed to provide users a choice of routes between parking areas, public streets or walkway systems, and building entrances and exits.

### Response

The proposed development incorporates building design and placement that will allow for a high level of visibility on the public spaces, outdoor amenity areas, walkways and paths and the streetscape. Appropriate lighting will be used throughout the development to permit high visibility and eyes on the street. Significant glazing will be incorporated into the buildings ground floor lobby, amenity areas and retail spaces to provide views into the street and other public areas.



Figure 14. Pickering Official Plan - Personal Security

## BARRIER-FREE ACCESS

### Policy

The following policies relate to Barrier-free access:

14.12 Community Council shall:

- (a) endeavour to ensure barrier-free access is provided to all public buildings, areas and transportation facilities by using features such as level surfaces, ramps, elevators, automatic doors, curb depressions, railings and rest areas;
- (b) endeavour to ensure that the main travelled portions of pedestrian routes are kept free of obstructions such as street furniture, signs or building projections; and
- (c) endeavour to ensure that barrier-free features are well integrated with the functional and aesthetic design of developments to preclude the perception of segregation.

### Response

The proposed development will be designed in accordance with AODA requirements to ensure barrier-free access for all.

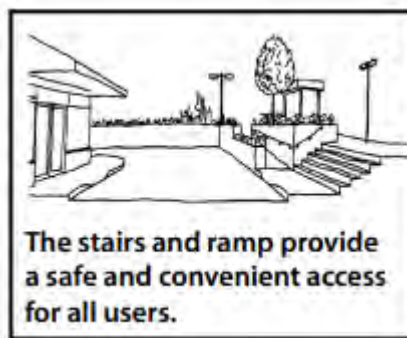


Figure 15. Pickering Official Plan - Barrier Free Access

## PUBLIC ART

### Policy

14.13 Community Council shall:

- (a) promote the placement of a range of art in publicly-accessible and visible locations such as parks, prominent street corners, plazas and on buildings;
- (b) encourage public art in a broad range of media, themes and formats in order to engage the observer, foster civic identity and promote social interaction; and
- (c) consider integrating public art in the early stages of the design and planning of developments.

### Response

Public Art will be included as part of the landscape design and be strategically located in the proposed development. The proposal currently includes a public art installation in the Gateway Plaza, in proximity to the intersection of Kingston and Dixie Roads. The exact design and placement of public art on the subject site will be refined during the planning process.



LIGHTING

Policy

Lighting will be utilized to enhance attractiveness and safety of the built environment. The following policies address lighting:

14.14 Community Council shall:

- (a) promote the use of lighting to enhance and define the aesthetic and functional quality of public places such as promenades, sidewalks, squares and parks;
- (b) promote the use of lighting fixtures that are compatible with the scale of pedestrian activity;
- (d) reduce the effects of light pollution on the night-time sky and on adjacent uses by requiring the use of lighting fixtures that are particularly suited to the purpose and setting in which they are to be utilized.
- (e) reduce the effects of light pollution on the night-time sky and on adjacent uses by requiring the use of lighting fixtures that are particularly suited to the purpose and setting in which they are to be utilized.

Response

Lighting for the proposed development will be designed to ensure pedestrian safety, to define public space and to emphasize and embellish the building design. Lighting will be used to demarcate building entrances and underground parking areas will also be well lit. Light fixtures and installations will be responsive to the local context and built environment. Lighting plans will be provided during the detailed design stage

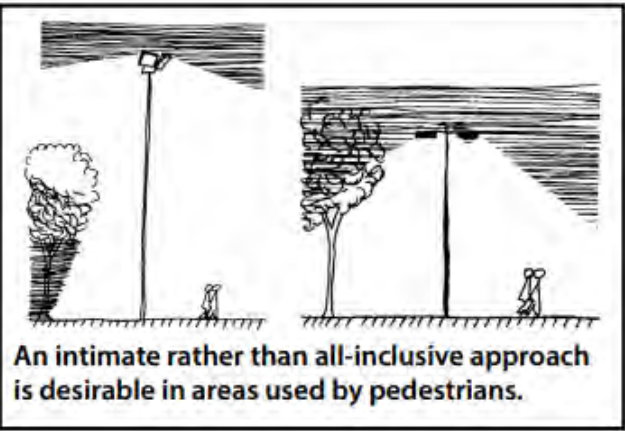


Figure 16. Pickering Official Plan - Lighting

SIGNAGE

Policy

14.15 Community Council shall:

- (a) require the design of signs to be used to enhance the appeal of developments, and to integrate with the architectural design of buildings, in order to contribute to the overall visual quality of the built environment;
- (b) encourage the use of an appropriate variety of signage types, such as fascia signs, canopies and awnings, projecting signs, ground signs, and directory signs, which complement building designs rather than dominate them;
- (c) encourage non-business related signs, such as directional signs, public information kiosks, and general identification signs, to be accommodated in the design of buildings that are adjacent to, and incorporate, public or publicly-accessible spaces; and
- (d) prohibit the use of portable signs except under specific circumstances and by permit only.

Response

Signage throughout the proposed development will be thoughtfully considered and appropriately incorporated into the building design.

## 2.3.6. Municipality of Pickering Zoning By-Law (2622/87, Parent By-law 3036)

### Policy

The Subject Site is currently zoned as M1-SC30 under Zoning By-law No. 3036, as amended by By-laws 4138/92 and 4361/93 (Figure 17). The M1-SC30 zone permits a range of commercial and employment uses. Buildings in the M1-SC30 zone have a maximum height of 12 metres. Additional zoning provisions are specific to the subject site related to parking, gross floor area, open storage, etc.

The immediate surrounding area is a mix of residential, commercial and. Lands on the north side of Kingston Road are a residential zones.

### Response

The proposed development does not conform to the M1-SC30 zone. As such a Draft Site-specific Zoning By-law Amendment including with this submission takes inspiration from the Pickering City Centre Zoning By-law, in order to allow for an intensified residential development on the subject site.

The Zoning By-law Amendment will facilitate the built form of the proposed development that will meet the general planning intent for the Kingston Road corridor, while having appropriate regard for the design direction provided by the Kingston Road UDG's adopted by Council.

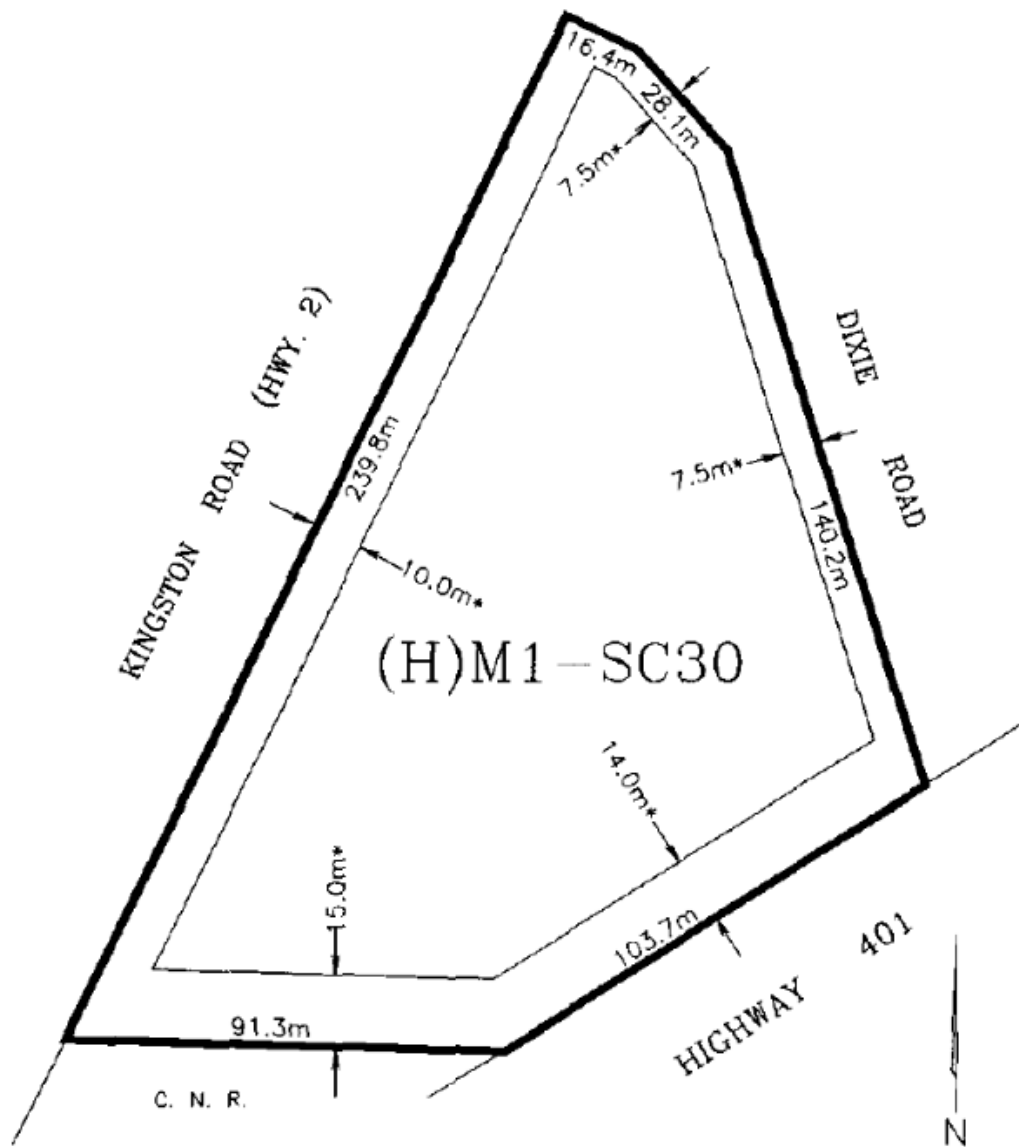


Figure 17: Municipality of Pickering Zoning By-law Schedule I

## 2.3.7. Kingston Road Corridor & Specialty Retailing Node Draft Urban Design Guidelines (2019)

### Dunbarton/Liverpool

The Subject Site is within the Dunbarton/Liverpool precinct (figure 18). The Dunbarton/Liverpool Precinct Built Form Plan is characterized by moderate to significant heights and densities throughout. Lands along Kingston Road are envisioned as mostly mid-rise in character, providing for transition between lower-rise established neighbourhoods to the north and the denser parts of the Precinct to the south. Greater heights and densities are located on the south side of Kingston Road, away from stable residential neighbourhoods to the north. The south side, of which the Subject Site is located, is made up of mid-rise and tall buildings.

As indicated Rougemount Precinct Built Form Plan (figure 18), primary frontages are proposed along Kingston Road for the Subject Site. The Subject Site is within the Gateway area of Altona Road and Kingston Road.

The following pages include analysis and responses to the policies within The Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines as they pertain to the Subject Site.

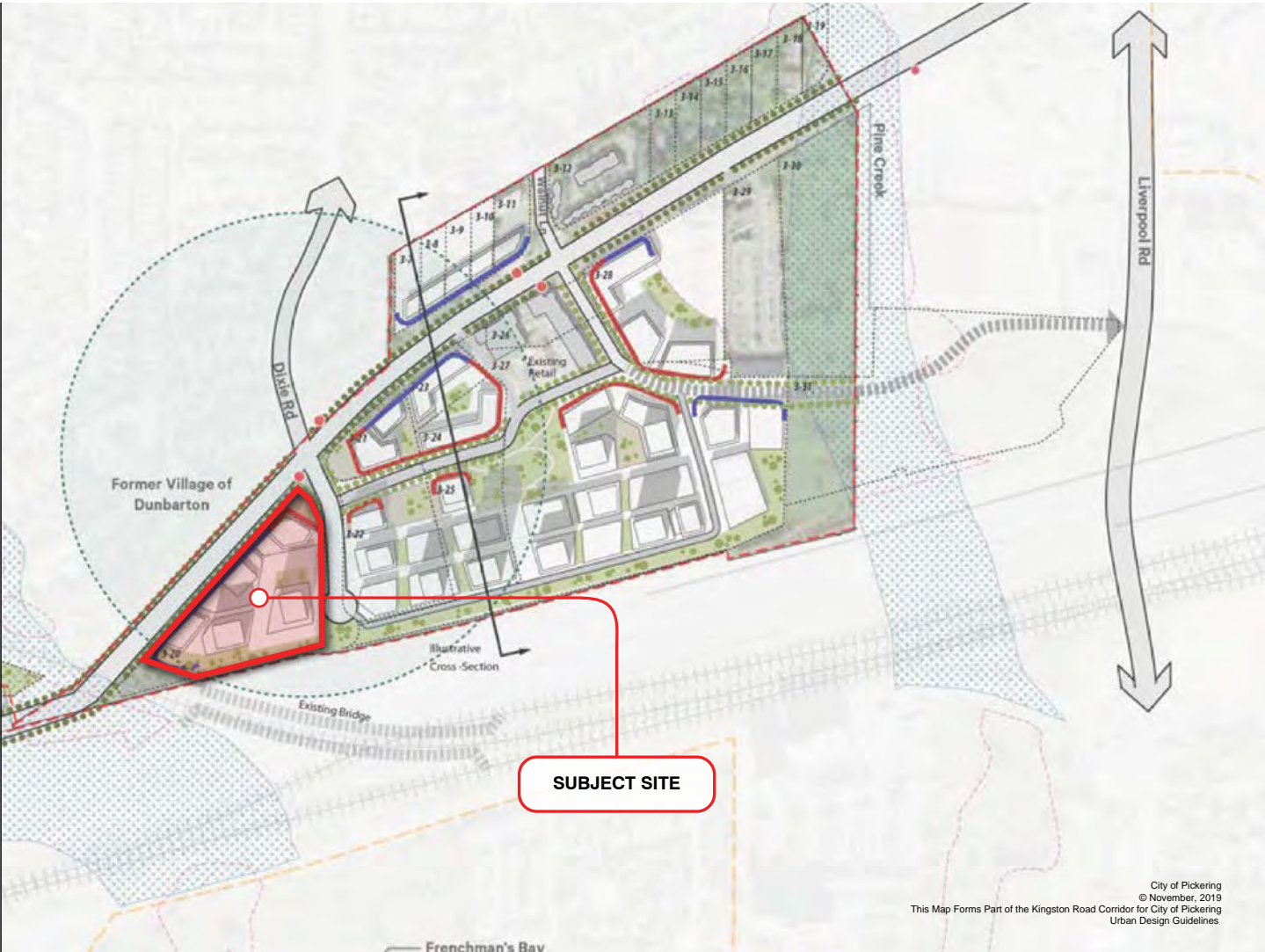


100m

Dunbarton/Liverpool Precinct  
Built Form Plan

Built Form

- Gateway
- Primary Frontage
- Secondary Frontage
- Existing Development Application
- Buildings and Shadows - Illustrative Only (March 21st at 1:00 pm)
- Landscape - Illustrative Only



City of Pickering  
© November, 2019  
This Map Forms Part of the Kingston Road Corridor for City of Pickering  
Urban Design Guidelines

Figure 18: Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines: Rougemount Precinct Built Form Plan

## Built Form Guidelines (2.0)

Section 2.0 Built Form contains policies with the following key objectives:

1. Promote higher-density mixed-use development while respecting the character and scale of established neighbourhoods;
2. Introduce an animated public realm; and
3. Retain and emphasize the distinct character of local streets and precincts, including heritage protection.

The Kingston Road Corridor and Specialty Retailing Node Urban Design Guidelines (Kingston Road UDG) includes a series of guidelines for the corridor and node to encourage a high-quality urban environment.

This document includes Built Form guidelines which inform the following:

### 2.2 Block Structure

- i) Blocks should range between 100 and 150 metres to promote permeability. Where a block is longer than 150 metres, mid-block connections should be introduced.

## Response

The proposal will be developed as one block with a frontage of 255.6 metres on Kingston Road. The development block has been divided into two podium buildings, each with two tower volumes above the eighth storey (Building 1) and sixth storey (Building 2). An at-grade mid-block connection between the buildings has been integrated to break up the large block and provide connectivity through the buildings and into the POPS and Multi-use Path.



## 2.3 Building Placement and Orientation

### 2.3.1 Building Entrances

i) Entrances should be highly visible and front onto public streets and connect to pedestrian walkways or sidewalks.

### 2.3.2 Building Separation Distances

v) Buildings over 13-storeys shall have a minimum separation distance of 25 metres between towers;  
vi) A minimum separation distance of 15 metres should be provided between adjacent buildings where windows are proposed within a podium. No sideyard separation is necessary where a continuous streetwall is desirable.

### 2.3.3 Building Setbacks

ii) In all precincts other than the Rougemont Precinct, buildings fronting Kingston Road, Brock Road and Pickering Parkway shall be setback 5 metres from the front property line.  
iii) Buildings fronting existing public roads intersecting Kingston Road shall be setback 3 metres in the Dunbarton/ Liverpool Precincts, or match the setback of adjacent buildings. In the case that the two adjacent buildings have differing setbacks, the new building setback shall match whichever is closer to the street.

## Response

Building entrances have been strategically located to front onto or in proximity to Kingston Road and Dixie Road. This placement will ensure high visibility and connectivity from the public realm. Tower volumes will be separated by a minimum of 25 metres. Podiums of Buildings 1 and 2 are separated by a minimum distance of 19.8 metres where residential units are present. Both Buildings have a continuous 5 metre setbacks from the property lines along Kingston Road and Dixie Road.

## 2.4 Grading and Access

ii) To minimize access off Kingston Road, consolidated private rear accesses should be provided. These should be developed with a coordinated approach across landowners to ensure that clear accessways are maintained, no properties are landlocked and all lots have a viable connection back to a public road;  
iii) Any redevelopment should seek to remove or minimize grade differences between its adjacent lots, including Kingston Road. Where this is not possible due to site topography, measures should be taken to make the transition walkable for pedestrians and accessible for cars;  
iv) Site grading shall consider facilities designed to provide access for persons with disabilities, including the provision of ramp access;  
v) Entrances and access points should be integrated with at-grade design;  
vi) Where possible, vehicular entrances and access points shall be located within the centre of the block and below grade with access from local streets/lanes; and  
vii) Vehicular entrances should have minimal impact on walkways and the pedestrian realm.

## Response

Residential accesses have been integrated into the site grading, as such, persons with accessibility concerns will be able to access all residential lobbies. Vehicular access will be provided from both Kingston Road and Dixie Road ensuring that all Buildings have access to a public road. Three curbs cuts are proposed for vehicular access points, minimizing conflict between road users. Parking level access will be provided via the internal network of drive aisles and will be largely shielded from the pedestrian experience.

## 2.5 Structured Parking

- i) Surface parking is discouraged for high-density residential developments. In these areas, parking shall be provided underground, behind or inside a structure on upper floors with appropriate screening, or inside a building;
- ii) Above-grade parking structures shall be encouraged to be designed with active uses on all sides;
- iii) The sides and rear of multi-storey above-grade parking structures facing adjacent developments shall be screened as to not create blank facades around the building. They are encouraged to incorporate glazing, cladding, landscaping, or exterior finishes to complement the surrounding streetscape;
- iv) At-grade parking structures shall be designed with active uses fronting the public street and other pedestrian uses, such as retail or amenity areas. These should incorporate visually-appealing architectural and landscape treatments;
- v) Access points to parking structures should be located at the rear or side of buildings, and away from main streets and intersection corners;
- vii) Structured underground parking is preferred over surface parking or above-grade structured parking to reduce the urban heat island effect and minimize blank walls; and,

viii) Consideration should be given to charging stations for electric vehicles and secure indoor bicycle storage space in the design of parking structures.

## Response

Parking is being provided in one level of below grade parking, as well as at-grade and above-grade parking contained within the podium levels of both Buildings. A pick-up/drop-off loop in the centre of the site will serve temporary parking uses at-grade. The proposed above-grade parking portions of the podiums have been wrapped with residential units, where possible. Portions of the parking podium that face the public realm will be treated with similar materiality than that of residential portions. This will guarantee a high level of architectural quality throughout the podium volumes and make the parking sections largely indistinguishable from residential portions. Vehicular access to parking are provided both at the side and rear of the property, minimizing impact on the public realm. A total of 92 parking spaces are expected to be electrified and another 40% are expected to be roughed-in. 895 bicycle spaces are provided internal to the parking structure.

## 2.6 Loading, Services and Utilities

- i) Where possible, on-site loading and servicing areas shall be located internal to the development and below grade with access from local streets and lanes. Access points shall be coordinated to minimize impacts on the pedestrian realm, including minimizing the interruption of sidewalks.
- iii) Service and loading facilities shall be contained within building envelopes and consolidated for each block, when possible. Below-grade loading facilities are encouraged for higher-density, larger-format development. Garbage storage rooms shall be centralized indoors, below grade, and at the rear of buildings.
- v) Utilities and service equipment shall be located within buildings or internal to building sites, where practical, to reduce their visual impact on the streetscape and public view. In outdoor areas, their presence can be minimized through screening, fencing, strategically-positioned landscaping and integration with public art.

## Response

The northern most access on Dixie Road provides direct the loading, waste pick-up and parking levels of Building 2. The southern access from dixie provides access to the loading, waste, pickup and parking of Building 1, as well as the drop-off loop between the two buildings. These spaces have been designed to have minimal conflict with vehicle and pedestrian movements. Required lengths, widths and overhead clearance are provided for the proposed loading spaces throughout the development. Loading spaces are appropriately located within building envelopes and garbage rooms are appropriately located and sized for the proposed number of units.

## 2.7 Landscape Design

- i) Landscape shall be an integral piece of the site design and be developed to unify and enhance the overall architectural project. High-quality, durable and diverse landscape elements shall be encouraged.
- iii) Landscaping shall support and define a consistent and attractive street edge. The selection and spacing of all plantings should relate to the street type and adjacent land use and site conditions; and,
- iv) Within sites, landscaping shall define pedestrian routes and enhance visual imagery of the site. Large tree canopies are encouraged along pedestrian routes to provide shade and comfort.

## Response

The proposed landscape design for the site proposes a boulevard-like approach to the Kingston Road frontage with tree planters, seating and arcade unit pavers. Through the site, trees have been proposed where soil volumes permit. The Gateway Plaza includes tree plantings, seating, public art and patterned pavers. The mid-block connection provides covered arcade walkways and tree planting in the centre of the pick-up/drop-off loop. The POPS to the south is to include trees and shrubs, bench seating, a dog run, as well as playground and water features. The southern edge of the site within the Controlled Access Highway will be plated with a buffer treatment.

## 2.8 Sustainable Design

i) Sustainable and Low Impact Development (LID) measures are encouraged for all development in order to reduce stormwater run-off and optimize water infiltration potential. This includes the use of bio-retention areas, rain gardens, grass swales, permeable pavement, and vegetated filter strips; ii) Development should prioritize plantings of native species that support ecological functions, are drought-tolerant, require minimal maintenance and increase biodiversity in the landscape; and, vi) The use of bird-friendly glazing on mid-rise and tall buildings is encouraged;

## Response

The proposed development has implemented Tier 1 requirements of the Pickering Sustainable Development requirements. Details related to planting species, energy modelling and bird-friendly design will be detailed at the Site Plan stage. Stormwater run-off is expected to be managed on site via the stormwater storage tanks, green roofs and porous portions of the landscape treatment.

## 2.9 Signage and Lighting

i) Signs should be clear, visible, and easy to understand. Signs should be properly lit to ensure safety on the road and walkways at night; vi) A dark-sky policy shall be promoted along Kingston Road with downward-directed lighting. All external light fixtures shall be full cut-off and dark-sky friendly to minimize sky glow effects and light pollution; and, vii) Pedestrian-scaled lighting shall be used for active public spaces, including inner-block walkways, parks, and courtyards.

## Response

Details regarding signage and lighting will be provided at the Site Plan stage, but will be developed to be in alignment with the guidelines, wherever possible.

## 2.10 Transition and Massing

- i) New buildings should be massed and scaled to establish compatible heights to adjacent streets and open spaces, while retaining a comfortable pedestrian scale;
  - iii) In cases where buildings have a height of 8 storeys or more proposed adjacent to the streetline, the upper storeys of the building should be sited on podiums having a minimum height of 3 storeys and a maximum height of 6 storeys;
  - iv) Development shall incorporate building and landscape design which minimizes the extent and duration of shadows and maximizes access to sunlight for adjacent low-rise developments, parks, open space, primary frontages, and other intensively used areas of the public realm;
  - v) The shadow impact of buildings on adjacent residential buildings, public parks and privately owned publicly-accessible spaces shall be assessed through a shadow impact study, where appropriate, and minimized to the extent possible;
  - iv) Development shall incorporate building and landscape design which protects and buffers the pedestrian realm from prevailing winds;
- To limit shadow and overlook impacts in low-rise residential areas, an angular plane shall be applied through the following:

viii) From the rear yard of low-rise residential (i.e. where development backs directly on to stable low-rise neighbourhoods), built form shall conform to a 45 degree angular plane measured from a height of 10.5 metres, set back 7.5 metres from the rear property line.

- To help create a human-scaled environment along public streets, an angular plane shall be applied through the following:

x) On Kingston Road, Brock Road, Pickering Parkway and existing north-south public roads intersecting Kingston Road, built form shall conform to an angular plane extended at a 45 degree angle from the front property line, beginning at a height 80 percent the width of the adjacent right-of-way.

xi) As an exception, on Kingston Road between Dunbarton Creek and Pine Creek in the Dunbarton/ Liverpool Precinct, built form shall conform to a 45 degree angular plane from the front property line, beginning at a height of 30 percent the width of the adjacent right-of-way.

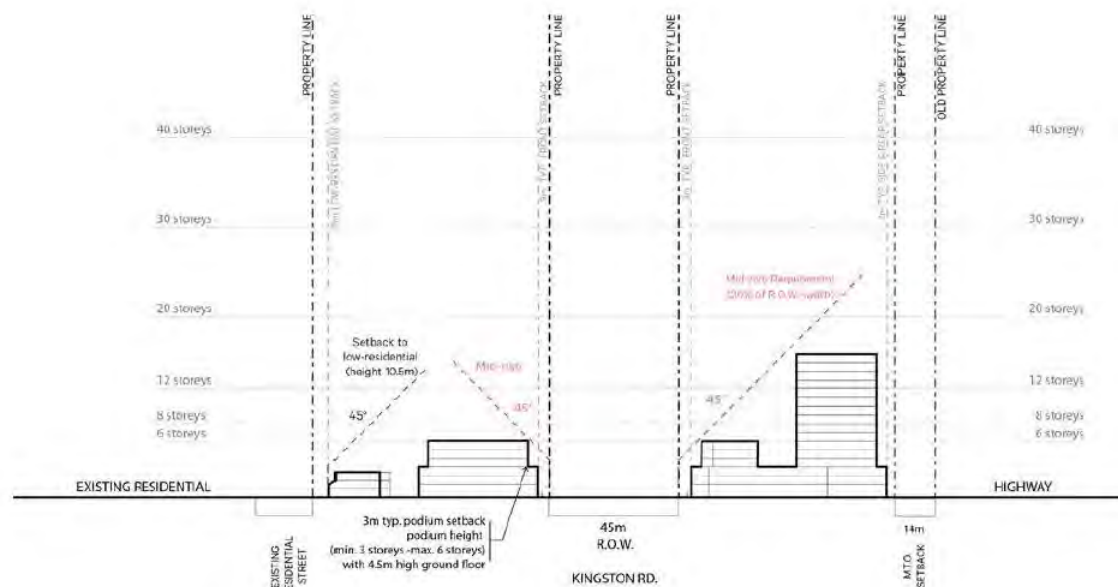


Figure 19. Cross-Section in Rougemount Precinct

## Response

The proposed development is located within the Dunbarton/Liverpool Precinct and contemplates development that is in line with the permitted density for gateways, in accordance with the OP. The proposed development is approximately 55 metres away from existing low-rise development, north of Kingston Road, and as such the proposed high-rise development is scaled and massed in a manner to mitigate impacts on existing neighbourhoods.

In accordance with the policies, the maximum podium height is largely complied with. Podium heights along the Kingston Road and Dixie Road frontages is planned at 6 storeys, in compliance with podium height policies. The Building 1 podium does reach 8-storeys along the podium volumes southern half. This elevated portion faces the rear of the site and has limited visual impact on the public realm. Additionally, the placement of this additional height is expected to have minimal additional impact on shadowing.

The guideline includes direction on applying a 45 degree angular plane from 30% of the right-of-way width (45 metres) as well as angular plane guidelines for parcels in proximity to established residential neighbourhoods. Based on the direction within the guideline as well as the policies of Chapter 11A of the Official Plan, the greatest heights and densities are to be directed to Gateways. As such, it is our opinion that applying an angular plane is incompatible with municipal goals related to height and density at Gateway sites. Pedestrians generally experience a buildings podium at the street level, rather than the towers. Given that our podium is within the angular plane and the towers are narrow and largely skewed towards a north south orientation, it is our opinion that the buildings heights of 35 storeys are appropriate.

A Shadow Analysis has been prepared by BDP. Quadrangle Architects Ltd. to help illustrate the shadow impacts by the proposed development. The Shadow Analysis has been included at the end of this document.

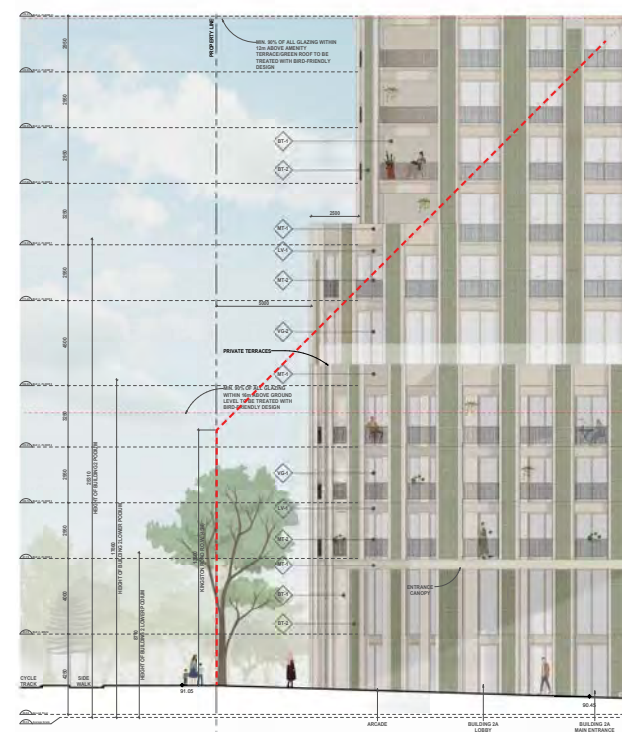


Figure 20. Angular Plane (BDP. QUADRANGLE ARCHITECTS LTD.)



## 2.11 Materials and Facade Treatment

- i) Main wall cladding materials should be high quality, aesthetically pleasing, and durable. Materials such as brick, stone and glass are encouraged;
- ii) Building materials that are discouraged include: stucco, vinyl, concrete block, metal siding, highly reflective glass and mirror finishes for glazing;
- iii) Materials should be complementary to the character of the precinct.
- v) Building materials for higher floors may differ from base materials, but compatibility, transition and building proportions should be considered. Higher buildings should have a lighter appearance in general to reduce perceived height, weight and bulk; and,
- vi) Facade articulation, including projections, recessions, design treatments and architectural details (i.e. decorative mouldings, fenestration, masonry banding) are encouraged to create enhanced visual interest and a human-scaled environment.

## Response

The enclosed coloured elevations and sections demonstrate the preliminary proposed materials and façade treatments of the built form, which are subject to further refinement. Details related to materiality, fenestration, and lighting will be fully detailed during the Site Plan stage. The project team remains committed to providing a consistent material expression along the podium volumes, regardless of their internal uses (residential vs. parking) to provide a pleasant pedestrian experience at grade.

## 2.12 Streetwall

- i) A consistent streetwall should be maintained along Kingston Road and all Primary Frontages;
- ii) The minimum streetwall height along all streets shall be 3 storeys;
- iii) The podium portion of tall buildings shall have a minimum height of 3 storeys and a maximum height of 6 storeys;
- iv) All street-related uses should have primary entrances fronting onto the public street and feature transparent windows and doors to provide outlook and animation onto the street edge;
- v) Generally, buildings shall have a podium of at least 3 storeys before any building stepbacks are introduced. The first stepback for any building, shall not occur higher than the sixth floor of a building;
- vi) Building stepbacks should be a minimum of 2.5 metres' and,
- viii) To introduce further variety and visual distinction within the streetwall, the establishment of façade articulation, differentiation and rhythm through building projections, recessions, and the use of distinct building materials is encouraged.

## Response

A comfortable streetwall condition that frames Kingston Road and Dixie Road is provided, with a consistent 6-storey podium at-grade experience along both thoroughfares. The podium massing is articulated with the highest section being strategically placed away from Kingston Road and Dixie Road to minimize its impact on the public realm. The Draft Zoning By-law Amendment permits podium heights of 9 to 33 metres. Building stepbacks between the tower and podium generally average 2.5 to 3 metres, largely in conformity with the Urban Design Guidelines.

## 2.13 Active Frontage Network

- i) Primary Frontages shall contain predominantly street-related active retail or commercial service uses at grade, with primary entrances oriented towards the street to encourage a vibrant public realm. Other street-related active uses, including community and institutional uses, are also permitted;
- ii) Secondary Frontages should contain street-related active retail or other commercial service uses at grade, with primary entrances oriented towards the street to encourage a vibrant public realm. Other street-related active uses, such as community and institutional uses, are also encouraged; and,
- v) A reasonable proportion of frontages shall have transparent windows at street level. Clear glass is preferred for all glazing in order to promote a high level of visibility.

## Response

The primary frontage of the proposed development is identified along the north eastern portion of the block at the intersection of Kingston Road and Dixie Road. Due to grading constraints, no active connections are currently proposed along this frontage. However, the architectural treatment of the primary frontage is expected to complement the adjacent Gateway Plaza by a visually appealing façade of high quality materiality and architectural expression. Multiple residential entrances are located along the Kingston Road frontage, promoting an active streetscape along the site's secondary frontage. Details on materiality and glazing will be provided during the detailed design stage.

## 2.14 Gateways

- i) Buildings with significant heights and massing should be located at gateway locations, including both mid-rise and tall buildings. Building and landscape design should aim to create a sense of arrival;
- ii) Gateways should incorporate public gathering spaces, such as plazas and urban squares;
- iii) Buildings at gateways are encouraged to include recessed corners to enlarge the public realm at key intersections to support additional spill-over space for active commercial uses;
- v) Building articulation, including vertical projections, recessions, design treatments and other architectural details, is encouraged at gateway locations to create enhanced visual interest and a distinct sense of place; and,
- vi) Heights, massing and articulation of buildings at gateways shall consider the aesthetics and orientation of view corridors approaching gateways to ensure a cohesive and prominent streetscape.

## Response

The proposed development is within the boundary of the Dixie Road Gateway and contemplates the inclusion of significant heights, up to 35 storeys. Podiums and towers have been positioned to serve as a landmark within the precinct as per policies and guidelines for Gateways.

## 2.15 Building Types

### 2.15.1 Tall Buildings

Tall buildings are generally defined as buildings that are 13 storeys or greater. Typically they contain active uses at grade with residential or office uses above.

i) Tall buildings should generally be located within gateways, including at the intersection of transit spines, major arterials, along the highway and proximate to highway access;

ii) Podiums shall have a minimum height of 3 storeys and a maximum height of 6 storeys to create a comfortable public realm. Towers should be stepped back a minimum of 3 metres from the podium wall;

iv) Tall buildings should be designed and sited to minimize shadows, maximize sky views, and reduce negative micro-climate impacts, particularly where high-rise development is directly adjacent to low-rise neighbourhoods, parks and open spaces;

v) Building towers shall be subject to a minimum 25 metre separation distance, measured between the exterior edge of the building face. Buildings shall have a maximum tower floor plate of 750m<sup>2</sup>;

vi) Upper floors should terminate the tower with distinctive crowning features and accent materials compatible with the overall building design; and,

vii) Building tops should incorporate screening for rooftop mechanical equipment to minimize their visual impact.

## Response

Tall buildings are to be located within gateways, including at the intersection of transit spines, major arterials, along highways and proximate to highway access. In accordance with the policies, the proposed development contemplates four towers with height of 35- storeys. Podiums will be experienced as 6-storey volumes by road users along Kingston and Dixie Roads. The slender tower floor plate of 750 m<sup>2</sup> has been designed to minimize shadow impacts where possible and maximize sky views. The towers are general oriented in a skewed north-south direction to further mitigate shadowing. A 25 metre minimum tower separation is achieved. The mechanical penthouse design is subject to further study and refinement.

## Place-making Guidelines (3.0)

Section 3.0 Place-making of The Kingston Road Corridor and Specialty Retailing Node Urban Design Guidelines includes guidelines and policies regarding the deliberate design of spaces to foster public interaction that bring cities to life.

The key objectives of the Place-making guidelines are as follows:

- Enhance and restore natural heritage features to strengthen their relation to adjacent uses
- Promote sustainability in the design and full life-cycle of the streetscape, open spaces and buildings
- Create a unique sense of place and distinct feeling of arrival for each precinct and throughout the overall corridor and node.
- Include high-quality urban environments with adversity of public spaces and community amenities.
- Contribute to overall place-making goals in support of creating complete communities

The Dunbarton/Liverpool Precinct Place-making Plan, as it applies to the subject site, identifies a Gateway Plaza on the subject site at the intersection of Kingston Road and Dixie Road. This Plaza is intended to support the future community's public open space needs. The proposal also includes a POPS at the southeastern edge of the property, adjacent to the proposed Multi-use Path.

This section provides responses to the relevant Place-making Guidelines

## 3.6 Gateway Plazas

- i) Gateway Plazas shall function as central gathering spaces which can be programmed for public or community events, and as pedestrian gateways and connections which complement the existing streetscape. The dimension, design and furnishing of these spaces should offer comfort and allow for a range of activities accommodating diverse user groups;
- ii) Gateway Plazas shall be physically and visually connected to the public street and well-designed to relate to surrounding buildings and create the impression of a cohesive public realm;
- iii) Gateway Plazas should be framed by adjacent streets, landscape and buildings which are designed to the highest architectural standard. They should respond to the form and function of the site and surrounding uses;
- iv) Commercial and mixed-use buildings adjacent to plazas should provide active frontages with direct views and access. Patios are encouraged to be located adjacent to these locations;

## Response

v) Gateways Plazas should contribute to a cohesive streetscape through the consistent use of colour, texture and building materials to the surrounding the built form; and,

vi) To create an enjoyable pedestrian environment, Gateway Plazas should incorporate appropriate lighting, signage, water features, and public art, where appropriate (Fig. 56). High quality paving treatments, in combination with landscaped elements including coordinated plantings and street furniture, should also be used.

The proposed Gateway Plaza is intended to function as a welcoming and flexible gathering space at the intersection of Kingston Road and Dixie Roads, adjacent to the future Bus Rapid Transit stops. The Gateway Plaza is to be framed by Kingston Road, Dixie Road and Building 2. While no active uses are planned in connection with the Gateway Plaza due to grade differentials, the Plaza will be designed with high quality landscaping and architectural finishes. The Plaza is expected to include seating and gathering opportunities, plantings (inclusive of shade providing trees) and public art to create a pleasant pedestrian experience at the Gateway to the Dunbarton/Liverpool Precinct. The exact materiality for the podium adjacent to the plaza and the landscaping treatment will be refined during the site planning stage.



### 3.8 Privately Owned Publicly-Accessible Spaces (POPS)

- i) POPS shall be publicly accessible, with signage to properly identify the space and indicate access for public use;
- iii) The size, shape and configuration of POPS will vary based on the existing and planned context and specific characteristics of the site and the building program;
- v) Private landowners shall be responsible for ongoing maintenance to ensure that POPS remain in a state of good repair through all seasons;
- vi) The location and design of POPS should seek to physically and visually connect to the public street;
- vii) POPS should be framed by and relate to surrounding buildings; at-grade active uses shall support the programming of the open space and offer a surveillance element to promote safety;
- viii) All POPS should incorporate soft landscape and planting; trees shall have sufficient soil volumes to enable large mature growth and a significant tree canopy;
- ix) POPS should maximize sun exposure and strive to achieve 5 consecutive hours of sun as measured on March 21 and September 21;

x) POPS should provide amenities including seating areas, pedestrian-scale lighting, bicycle racks, garbage cans, and public art to create a positive walking and cycling environment. Amenities should compliment the character of the surrounding public realm and active ground floor uses; and,

POPS designed as Parks should:

- xi) Be located to provide areas of open green space where intensified development is expected or planned to occur.
- xii) Have a dimension of a minimum of 0.2 ha, with larger spaces preferred.
- xiii) Include seating areas, walkways, a playground with junior children's play equipment, an open turf area, and tree canopy.

### Response

The proposed POPS goes above the identified Place-making attributes of the Dunbarton/Liverpool Precinct. The POPS has been designed to engage with Dixie Road and be highly visible from the cul-de-sac. Proposed POPS programming includes areas with soft landscaping as well as programmed areas including a dog run, a playground, a water feature and generous bench seating. The POPS' location along the southern edge of the site will increase access to sunlight, providing a pleasant environment year-round. The POPS complements the proposed Multi-use Path by having programmed space for Path users to rest and socialize in.

### 3.9 Public Art

- i) Public art should be located in or with close proximity to community-oriented spaces, such as parks, open spaces, public squares, plazas, and gateways, to maximize visibility. It should be exhibited along streets and laneways that support a continuous flow of high pedestrian volumes.
- ii) Public art should be durable and low-maintenance.

### Response

A public art installation is proposed within the Gateway Plaza, supporting the place-making of this Gateway space. The installations approach and materiality will be defined through the detailed design stage.

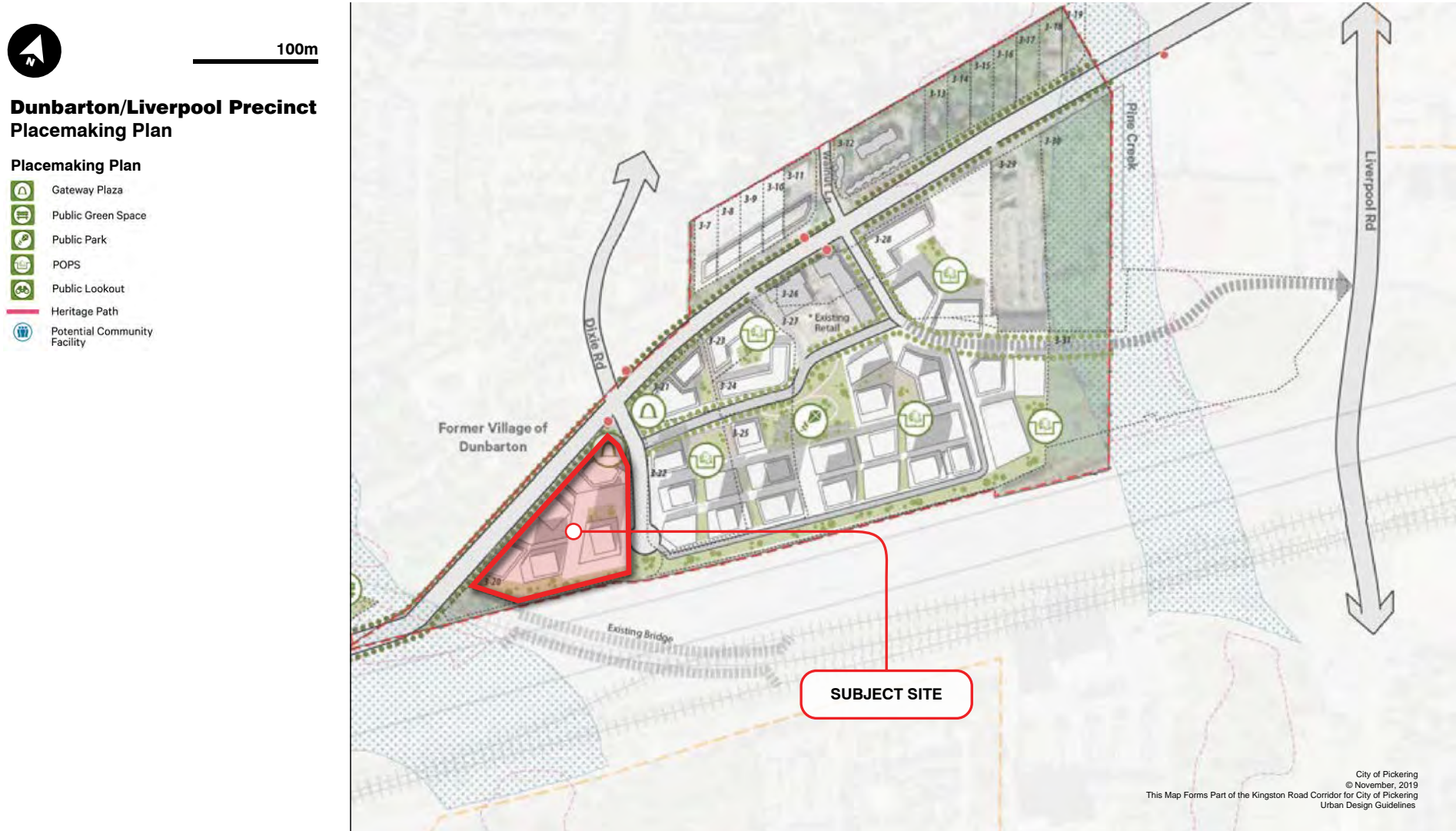


Figure 20: Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines: Rougemount Precinct Place-making Plan

## Connectivity Guidelines (4.0)

Section 4.0 Connectivity includes guidelines that will inform the creation of an integrated mobility network that takes into account the needs of pedestrians, cyclists, transit riders and drivers.

The key objectives of the Connectivity guidelines are as follows:

- Design all public roads and private connections to be complete streets and emphasize transit and pedestrian-oriented development.
- Improve access management and connectivity for all transportation modes.
- Ensure that all users of have distinct and delineated spaces to separate modes of travel moving at different speeds.
- Support current and future transit services through building and site design and public streetscape treatments.
- Encourage the optimization of existing and planned infrastructure, including transit facilities.

The Dunbarton/Liverpool Precinct Connectivity Plan does not identify any new streets on the subject site. A planned cycling network runs along Kingston Road as well as along Dixie Road south of Kingston Road. The proposal does not include dedicated cycling facilities on-site but is supportive of future cycling infrastructure in the right-of-way width by means of the provided bicycle parking spaces. The planned Durham-Scarborough BRT runs along Kingston Road, with a planned stop at Dixie Road next to the subject site. No road widening requirements have been identified on the subject site to allow for the final road structure inclusive of the BRT and cycle lanes.

The following sections provide responses to the relevant Connectivity Guidelines.

## 4.2. Pedestrians

### 4.2.1 Sidewalks

- i) Sidewalks should provide a network of accessible and inter-connected pedestrian routes which relate directly to surrounding buildings and destinations;
- ii) Sidewalks should provide a clear, unobstructed pathway and be a minimum width of 2 metres to ensure a comfortable walking environment;
- iii) Sidewalks should be designed to serve all users, including children, older people, parents with strollers, the visually impaired, and those using wheelchairs and other assistive devices. Barrier-free surfaces should be in compliance with Accessibility for Ontarians with Disabilities Act (AODA) standards;
- vi) Adequate space should be provided within the public right-of-way to allow for landscape and furniture zones adjacent to sidewalks; and,
- viii) Where appropriate, street trees which provide significant canopy shading should be planted to soften the built form, reduce the heat island effect and maximize the urban tree canopy. Trees should be incorporated at intervals of 6 to 9 metres.

## Response

The proposal includes new sidewalks and pedestrian walkways around the perimeter of the site along Kingston Road and Dixie Road, as well as through the mid-block connection. These walkways will meet the minimum width standards and be designed with accessibility in mind. Complementary furniture, including benches and seating, will be provided strategically along Kingston Road. Where soil volumes allow, street trees will be planted, contributing to the urban street canopy.





100m

## Dunbarton/Liverpool Precinct Connectivity Plan

### Legend

- New Streets
- Potential Controlled Intersection and Pedestrian Crossing - Location Subject to Further Review
- Indicative Enhanced Boulevard
- ↗ Right-In, Right-out
- ↘ Access
- Multi-Use Path
- Proposed Cycling Facility
- Existing Cycling Network
- - - Planned Cycling Network
- <---> Pedestrian Path
- Bus Stops
- <---> Future & Planned Connection Subject to EA

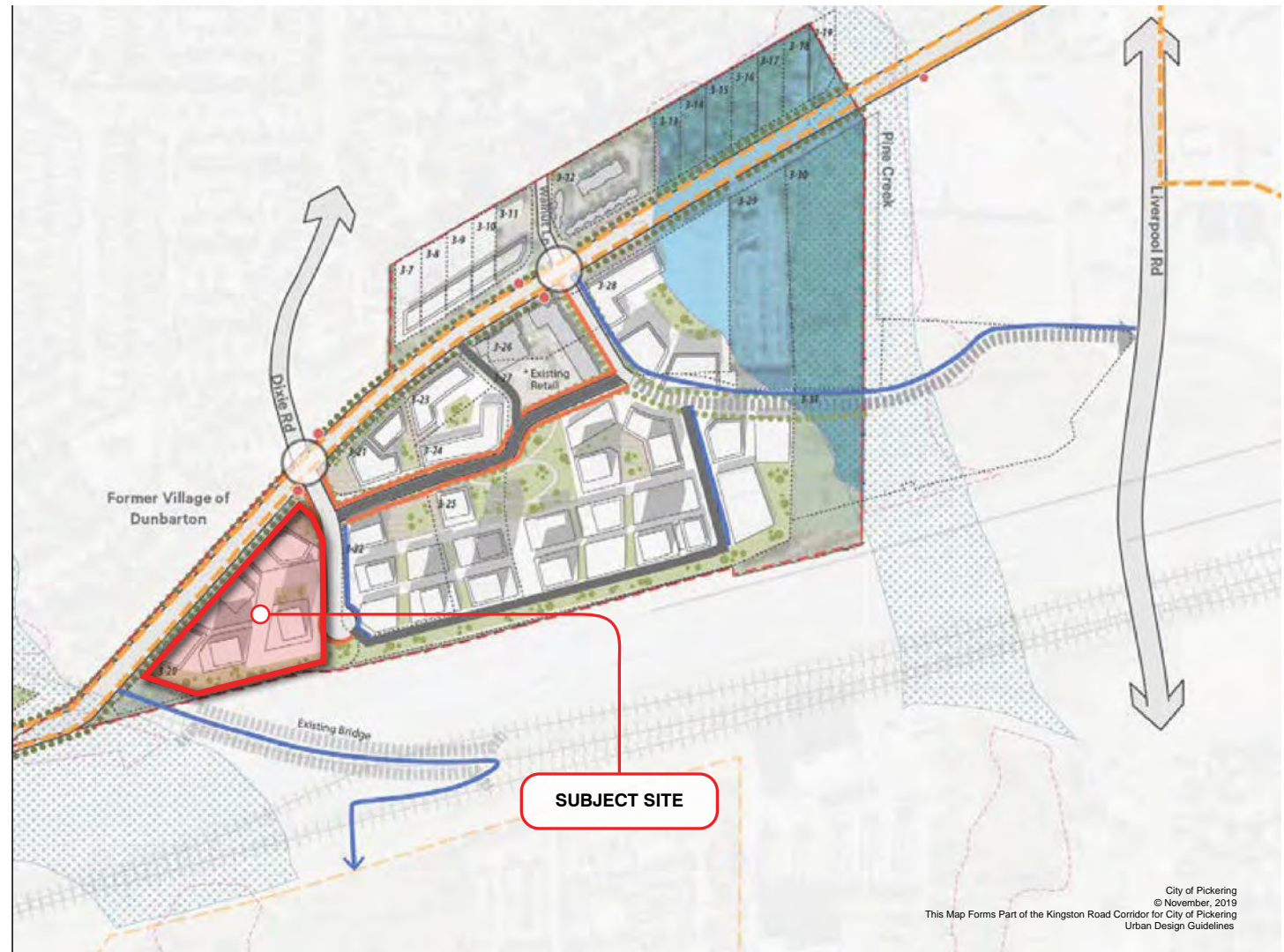


Figure 21: Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines: Rougemount Precinct Connectivity Plan



#### 4.3. Cycling

##### 4.3.1 Multi-Use Paths

- i) MUPs are encouraged as connectors between neighbouring communities, transit corridors and nodes.
- ii) When appropriate, multiple access points should be provided to all MUP, with connections to a variety of transportation options including public transit routes, other separate cycling facilities and MUP, sidewalks and parking areas.
- vii) MUP should include adequate amenities, such as seating, waste receptacles, lighting, signage, wayfinding features, and educational and historic information. These features should be located at accessible key points along path routes.

#### Response

The proposed multi-use Path runs the length of the subject site in a general east-west direction along the site's southern edge, away from the boulevard. The Path has a general width of 6 metres and will provide connectivity from Kingston Road to Dixie Road. A Multi-use Path proposed to the east of the site through the Tribute Lands will provide additional connectivity to future commercial uses and further east via the Walnut Lane extension towards the City Centre. The design of the Path including materiality and amenities will be refined through the planning and development process.

## 4.5. Street Types

### 4.5.3 Service Streets and Laneways

- i) Service streets and laneways should be considered in key areas where a street-oriented built form with continuous active frontages is desired, to allow for buildings to be placed closer to the street edge so that servicing functions can be allocated at the rear of properties.
- ii) Travel lanes should be designed with a minimum width of 3.5 metres and should be provided in one or both directions of travel.
- iii) A sidewalk should be provided on one side of a service street or laneway.
- iv) The use of permeable surface materials is encouraged within service streets and laneways.
- v) Service streets and laneways should be considered as pedestrian corridors, and should be designed with the pedestrian experience in mind. Where appropriate, the rear façade of buildings should be similar in quality (i.e. materials, articulation) to the front façade.

vi) Where possible, soft landscaping should be incorporated into the design of service streets and laneways. Planters, shrubs and vegetation strips are encouraged.

## Response

The new driveways providing access to the site from Kingston Road and Dixie Road will be designed in alignment with design guidelines for Service Streets and Laneways. This will include wider lanes, sidewalks, and soft landscaping where possible. The inclusion of private driveways allow for the centralization of parking and servicing uses, guaranteeing a continuous active streetwall along existing roadways.



### Dunbarton/Liverpool Precinct Street Types Plan

**Connectivity - Streets**

- Primary Streets (Public)
- Primary Streets (Private)
- Secondary Streets
- Service Streets
- Potential Controlled Intersection and Pedestrian Crossing - Location Subject to Further Review
- Indicative Enhanced Boulevard
- Right-In, Right-out
- Access
- Future & Planned Connection Subject to EA



Figure 22: Kingston Road Corridor and Specialty Retailing Node Draft Urban Design Guidelines: Rougemount Precinct Street Types Plan

## 2.3.8. Kingston Road Corridor & Specialty Retailing Node Intensification Plan (2019)

### Policy

The Kingston Road Corridor and Specialty Retailing Node Intensification Plan (“Intensification Plan”) was endorsed in principle by City Council in December of 2019 and informed Official Plan Amendment 38. This document sets out a detailed plan for desired land use mix, built form, and area character. Intensification Plans for several areas are included, including the Dunbarton/Liverpool Precinct in which the subject site is located. Similarly to the UDG described in section 2.3.7 of this document, the Intensification Plan includes policy and guideline based recommendations which direct Land Use, Built Form and Place-making. Significant overlap between the Urban Design Guidelines discussed in section 2.3.7 and the Intensification Plan can be observed.

### Response

The proposed development is consistent with the Kingston Road Corridor Intensification Plan in transforming Kingston Road into a “main street”. Please see section 2.3.7 for a comprehensive discussion on the urban design approach to this site in relation to relevant policies and guidelines.





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# **Dunbarton/Liverpool Precinct Intensification Plan**

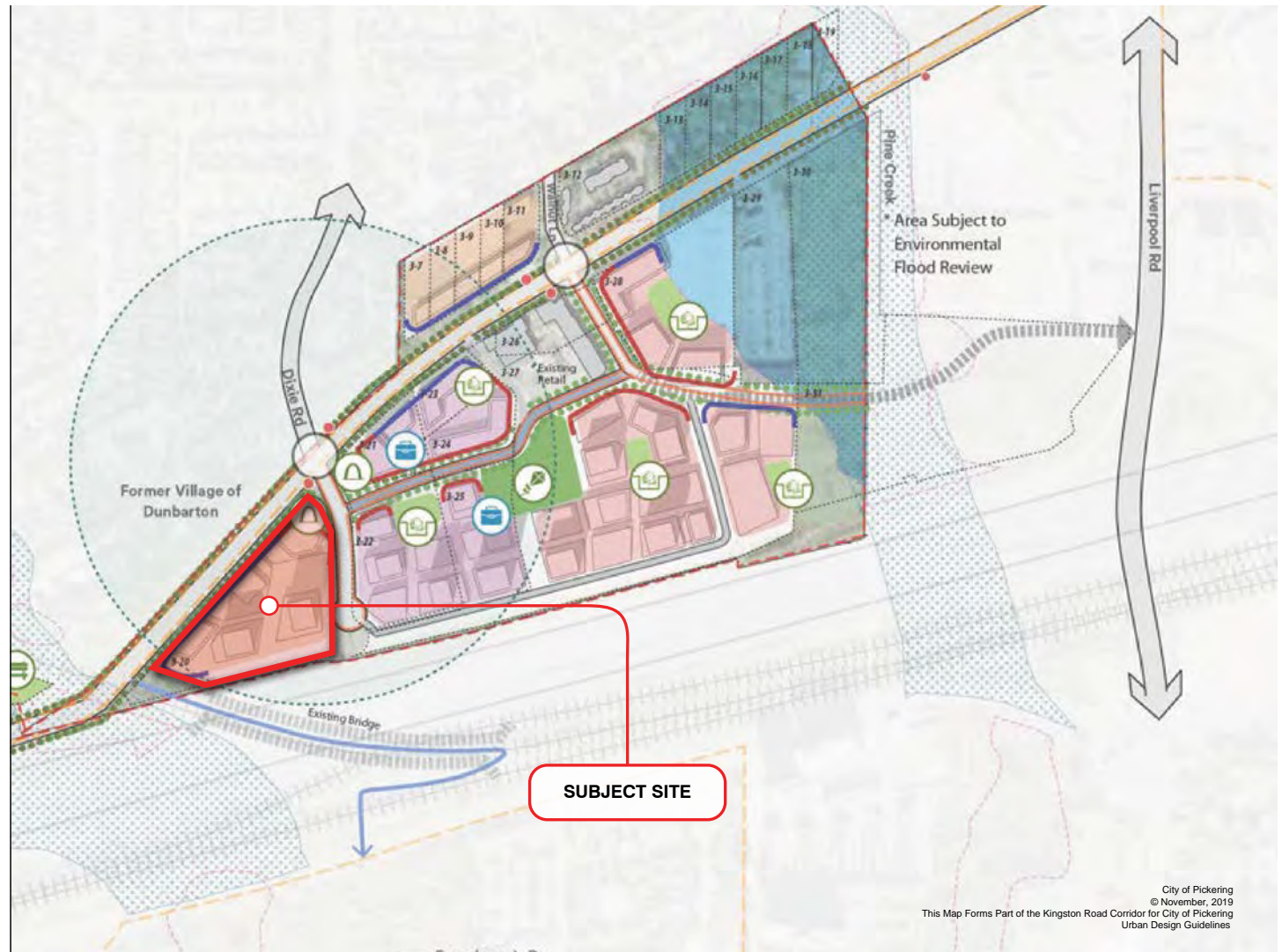


Figure 23: Kingston Road Corridor and Specialty Retailing Node intensification Plan - Dunbarton/Liverpool Precinct Intensification Plan

# 3 THE PROPOSAL







Figure 25. Aerial Rendering (BDP. QUADRANGLE ARCHITECTS LTD.)



## 3.1. Development Proposal

### PROPOSED DEVELOPMENT

The proposed development introduces four high-rise towers across two buildings that will offer a mix of residential uses. The proposed towers are 35-storeys in height on podiums ranging between 4-storeys to 8-storeys. The proposed development will have 1,492 residential units. The Site has an overall net floor area of 98,184 sq.m, a net site area of 20,834 sq. m, and an FSI of 4.71.

A total of 920 long term residential parking spaces and 223 visitor parking spaces provided over eight levels. Parking will be provided in one below grade level and in seven above-grade podium levels. Access to parking, servicing and loading is provided from two access points on Dixie Road. A third access right-in-right-out vehicle access is located on Kingston Road. A total of 746 long-term and 149 short-term bicycle parking spaces are provided. There will also be provision for 111 long-term electric bike parking spaces.

The above-grade podium parking will result in residential units sitting at a higher elevation, thus providing better views to Frenchmen's Bay to the south and providing better separation between residential units and the railway tracks and highway 401 traffic.

The proposed development provides a total of 5,985 square metres of amenity space. 3,000 square metres of indoor amenity space is provided and 2,985 square metres of outdoor amenity space on levels 1, Mezzanine, 5, 7 and 9.

A 14 metre wide setback is provided along the south and west property line from highway 401.



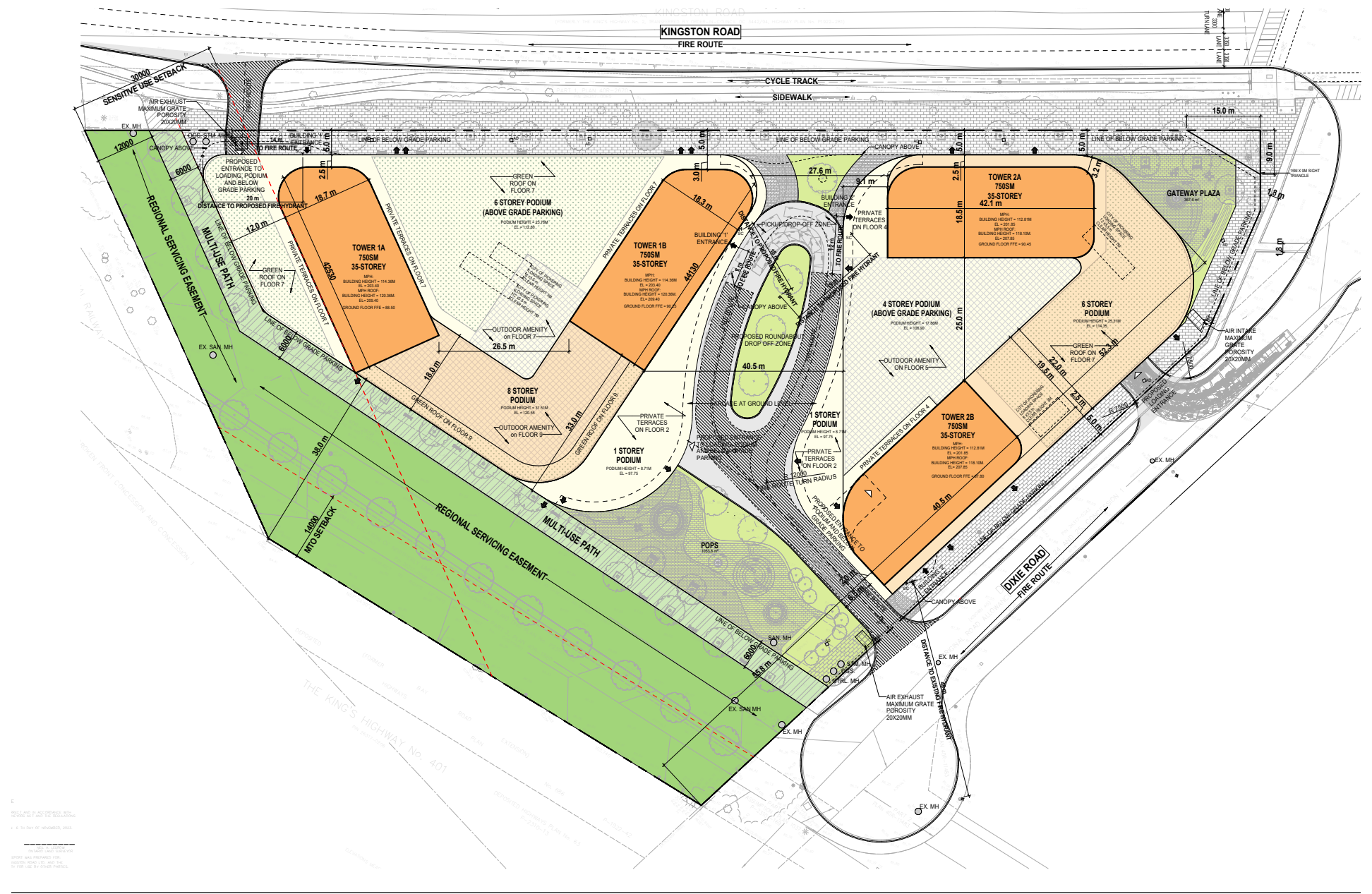


Figure 26. Site Plan (BDP. QUADRANGLE ARCHITECTS LTD.)





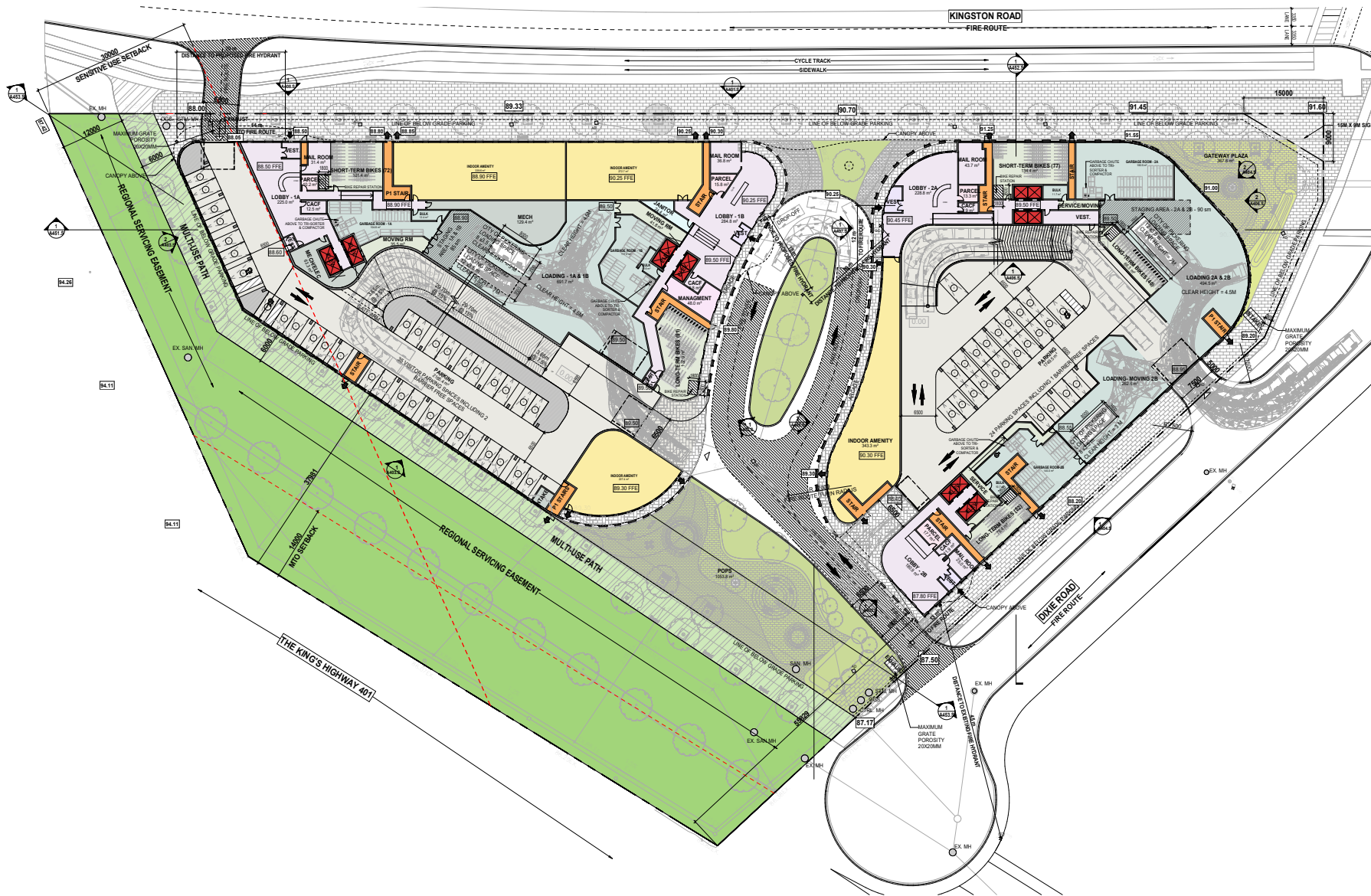


Figure 28. Ground Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)

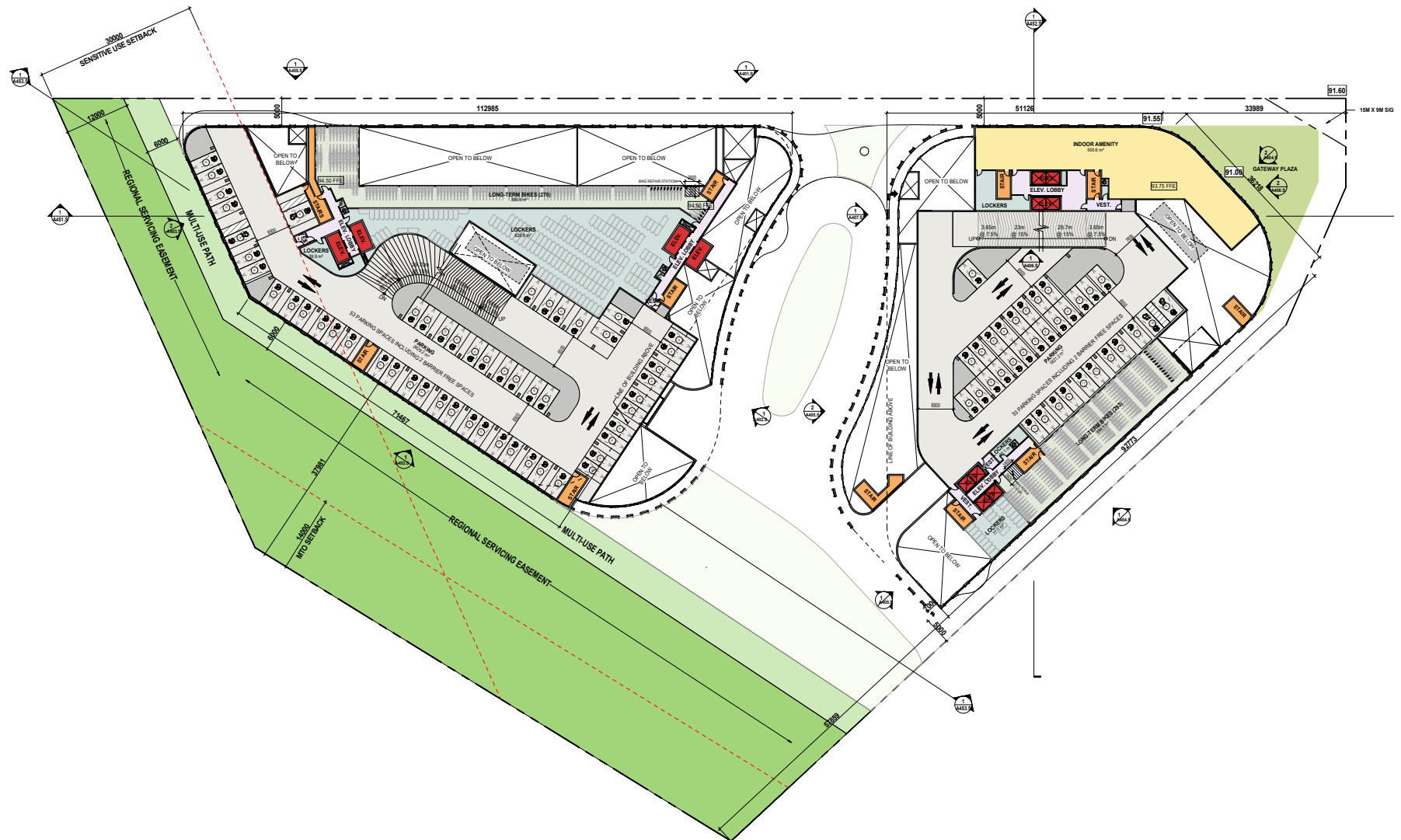


Figure 29. Mezzanine Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)



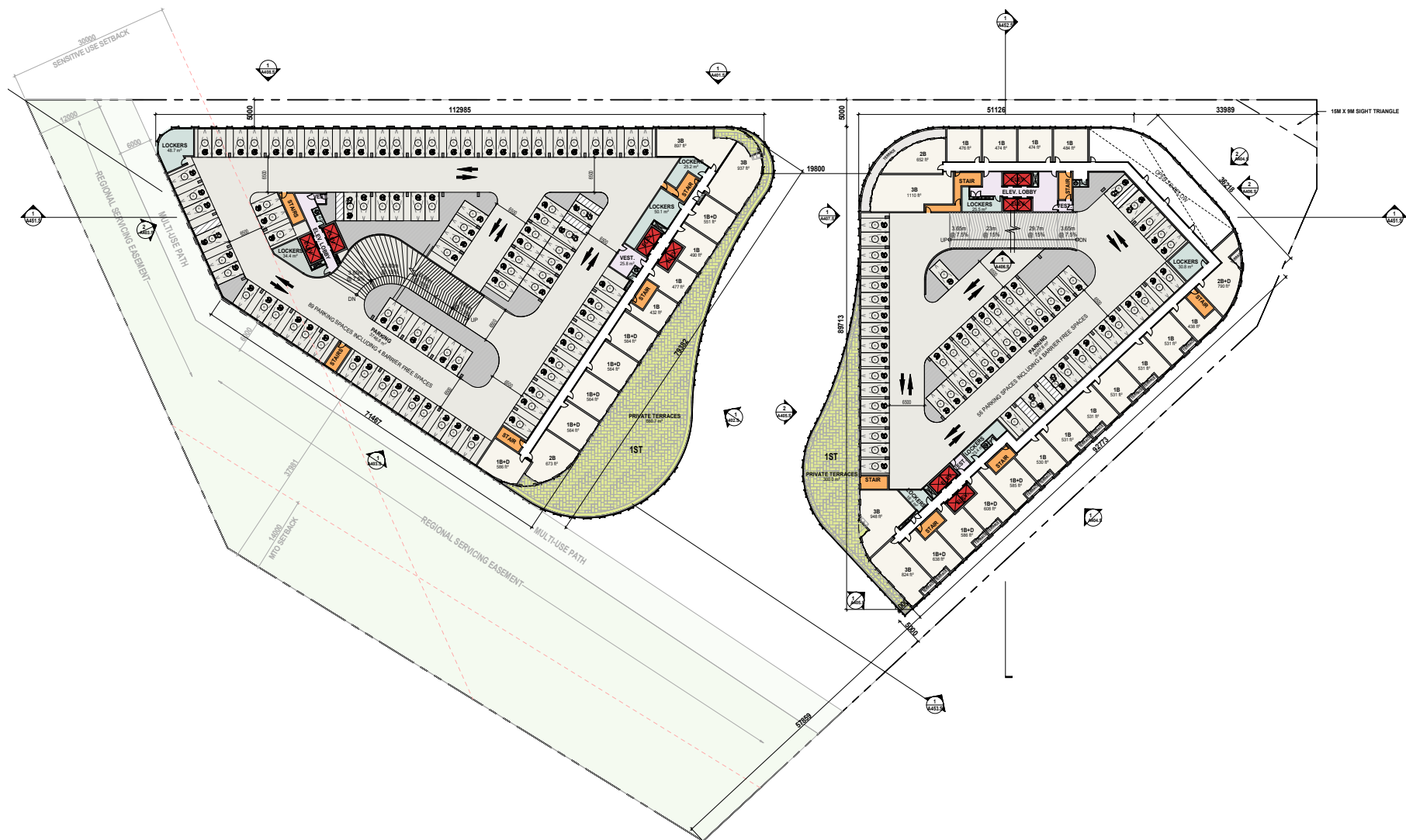


Figure 30. Level 2 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)

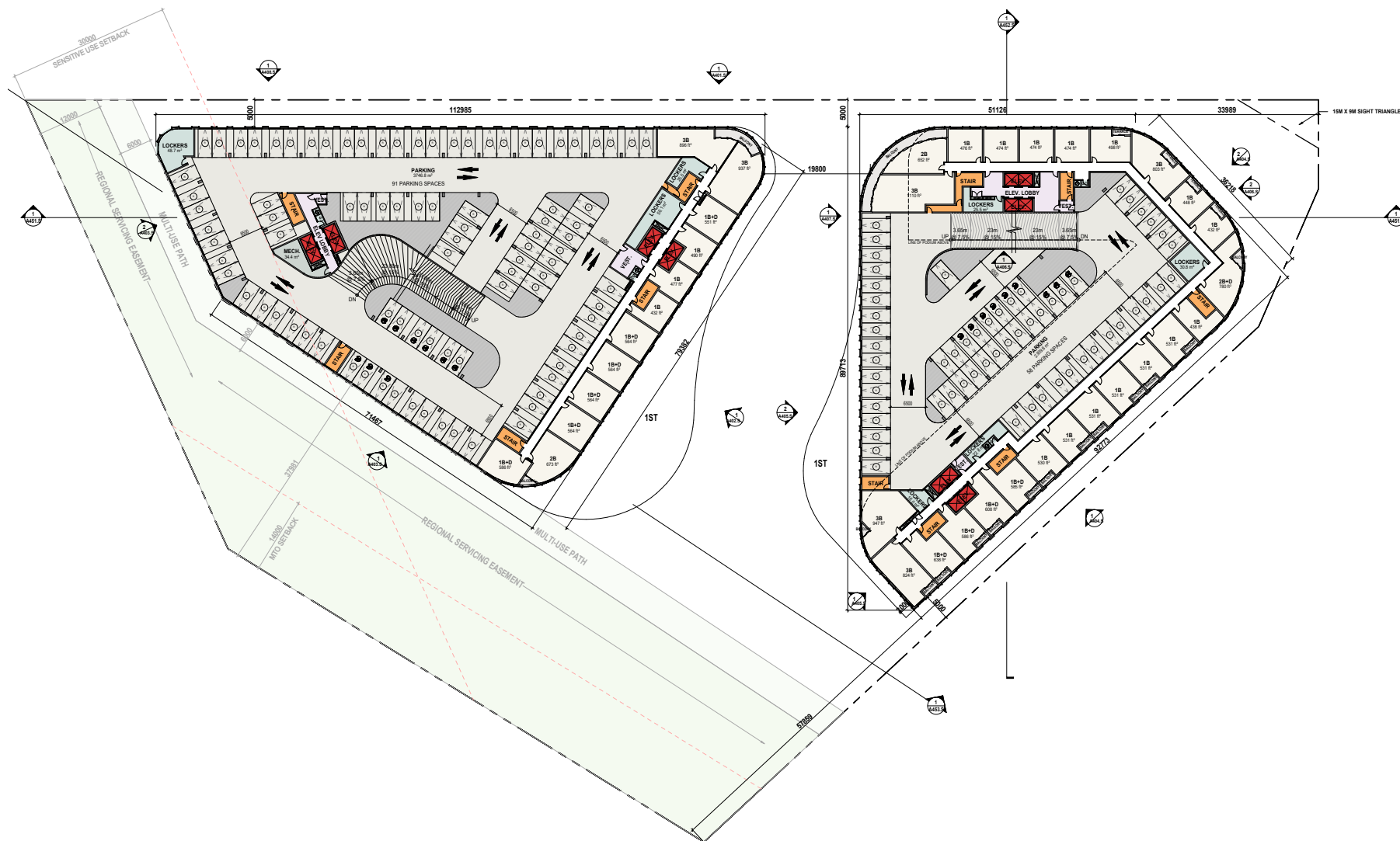


Figure 31. Level 3-4 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)



Figure 32. Level 5 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)



Figure 33. Level 6 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)

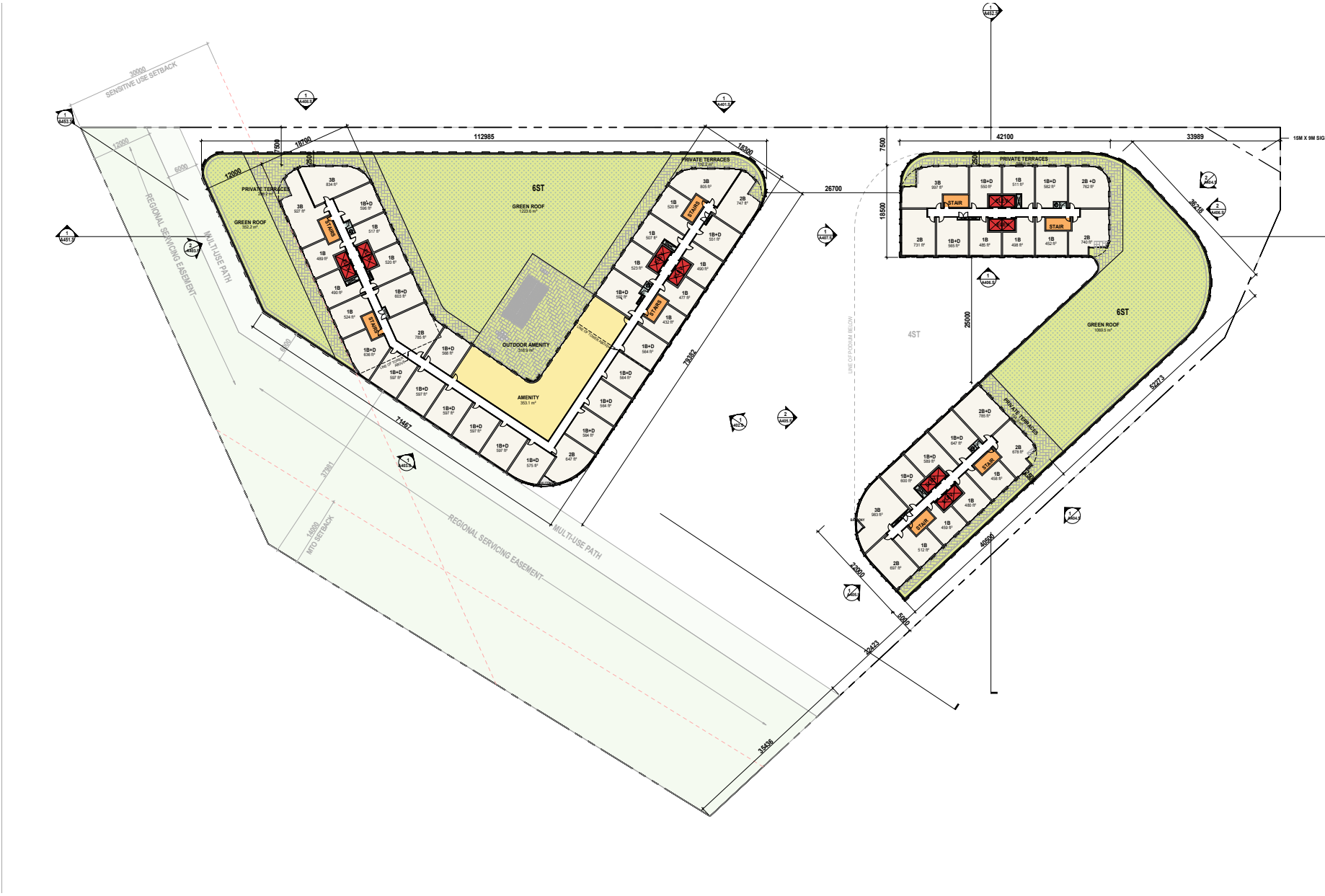
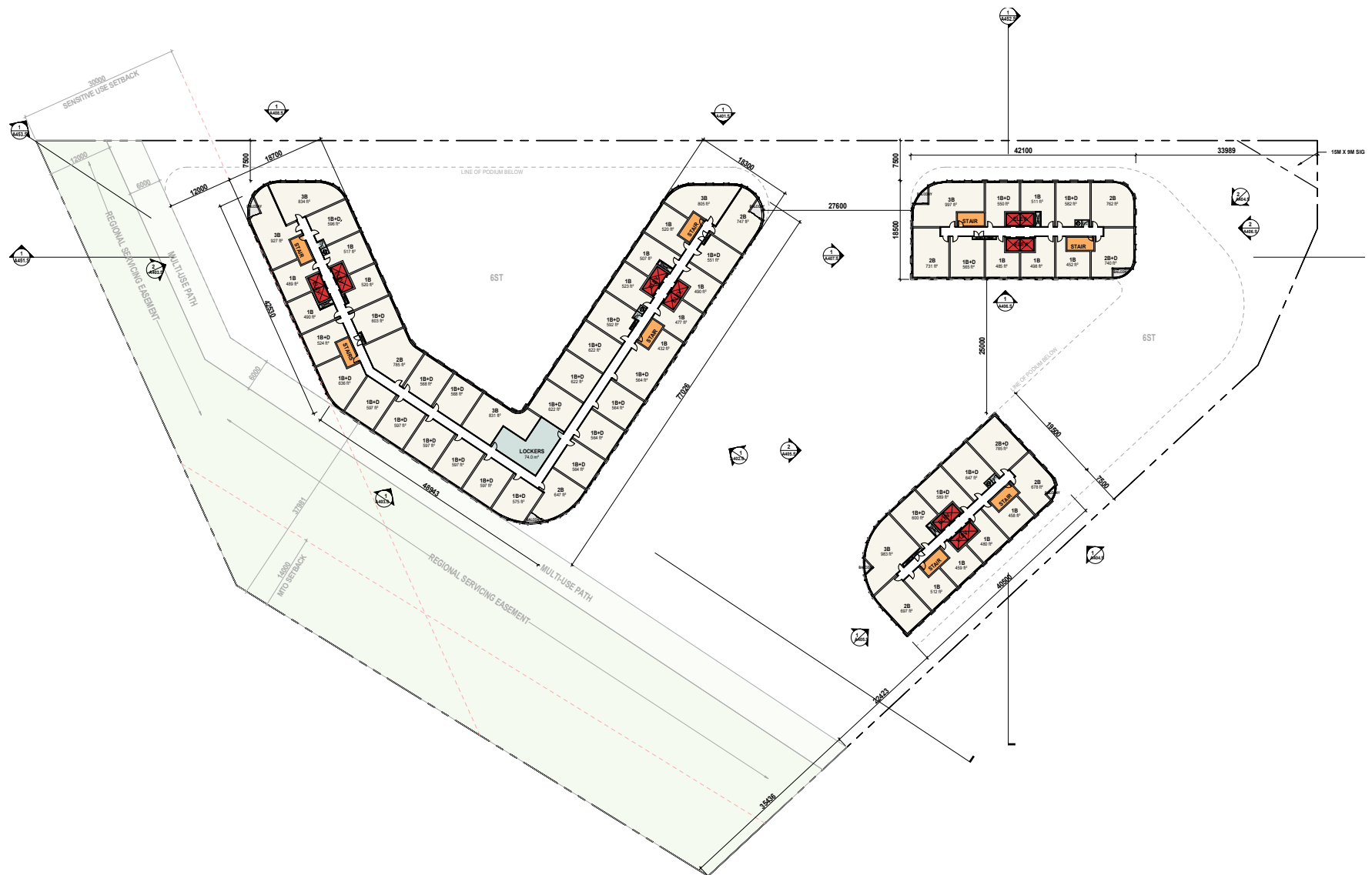


Figure 34. Level 7 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)





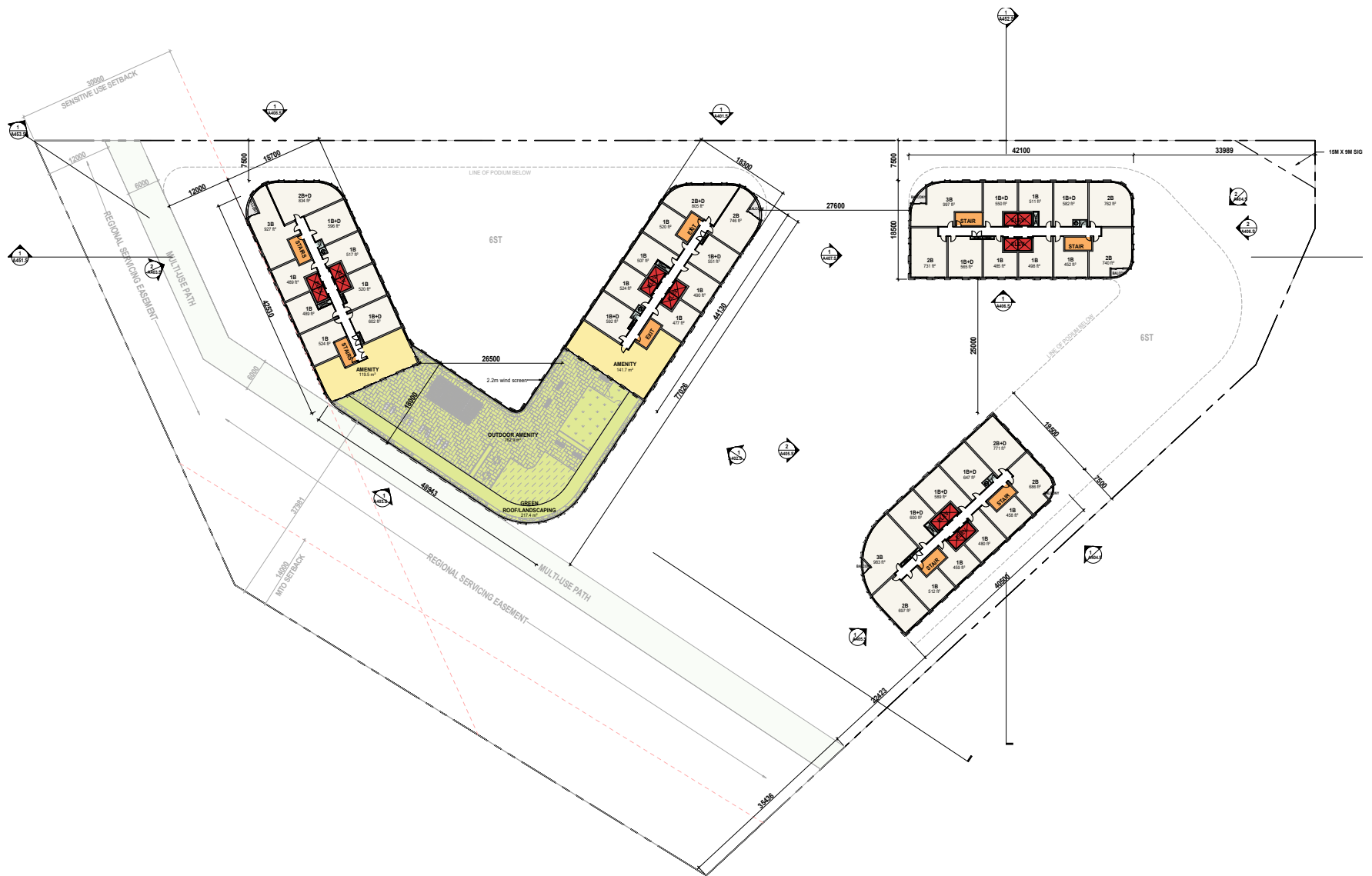


Figure 36. Level 9 Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)

83

Figure 38. MPH Floor Plan (BDP. QUADRANGLE ARCHITECTS LTD.)

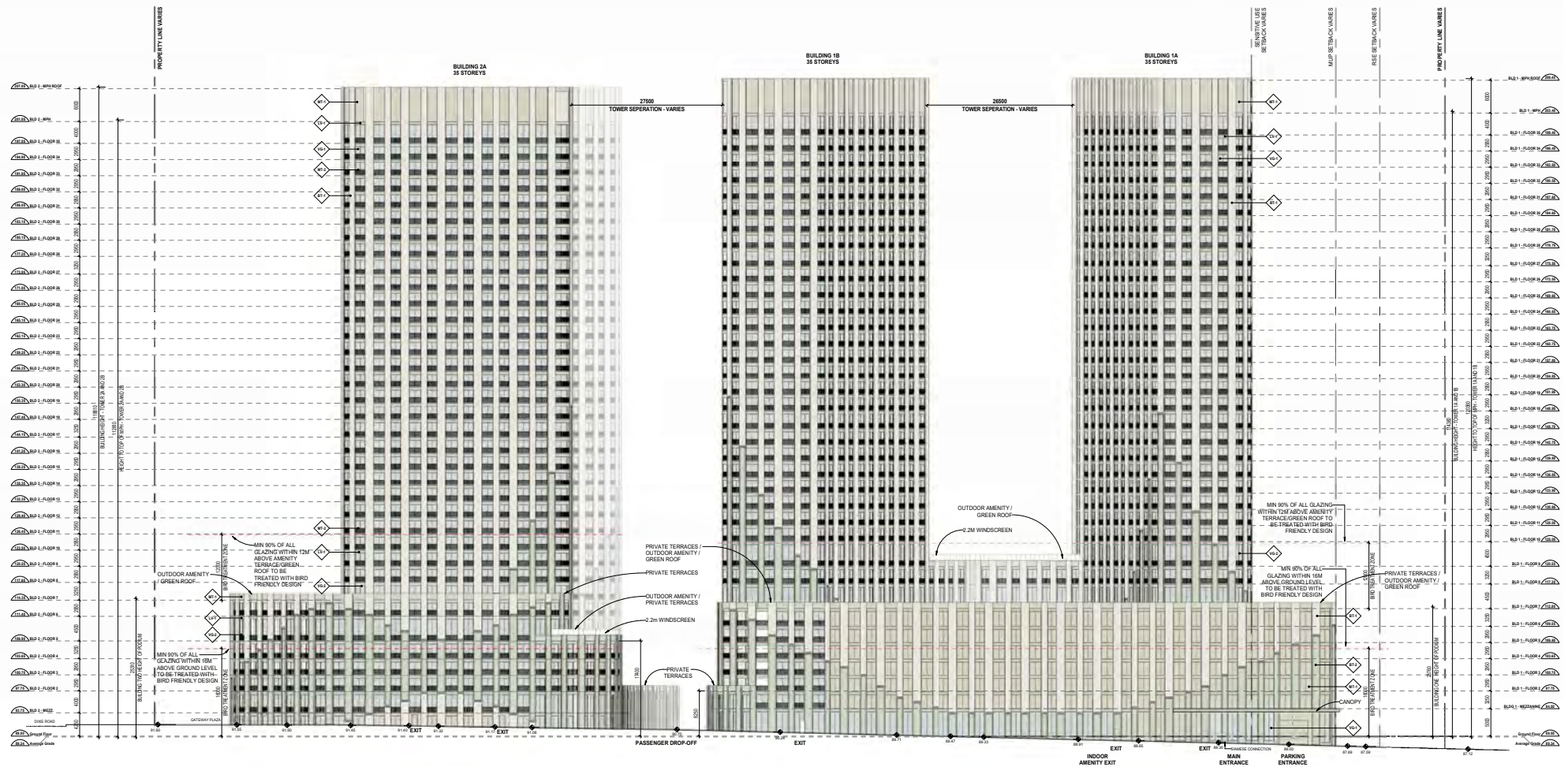
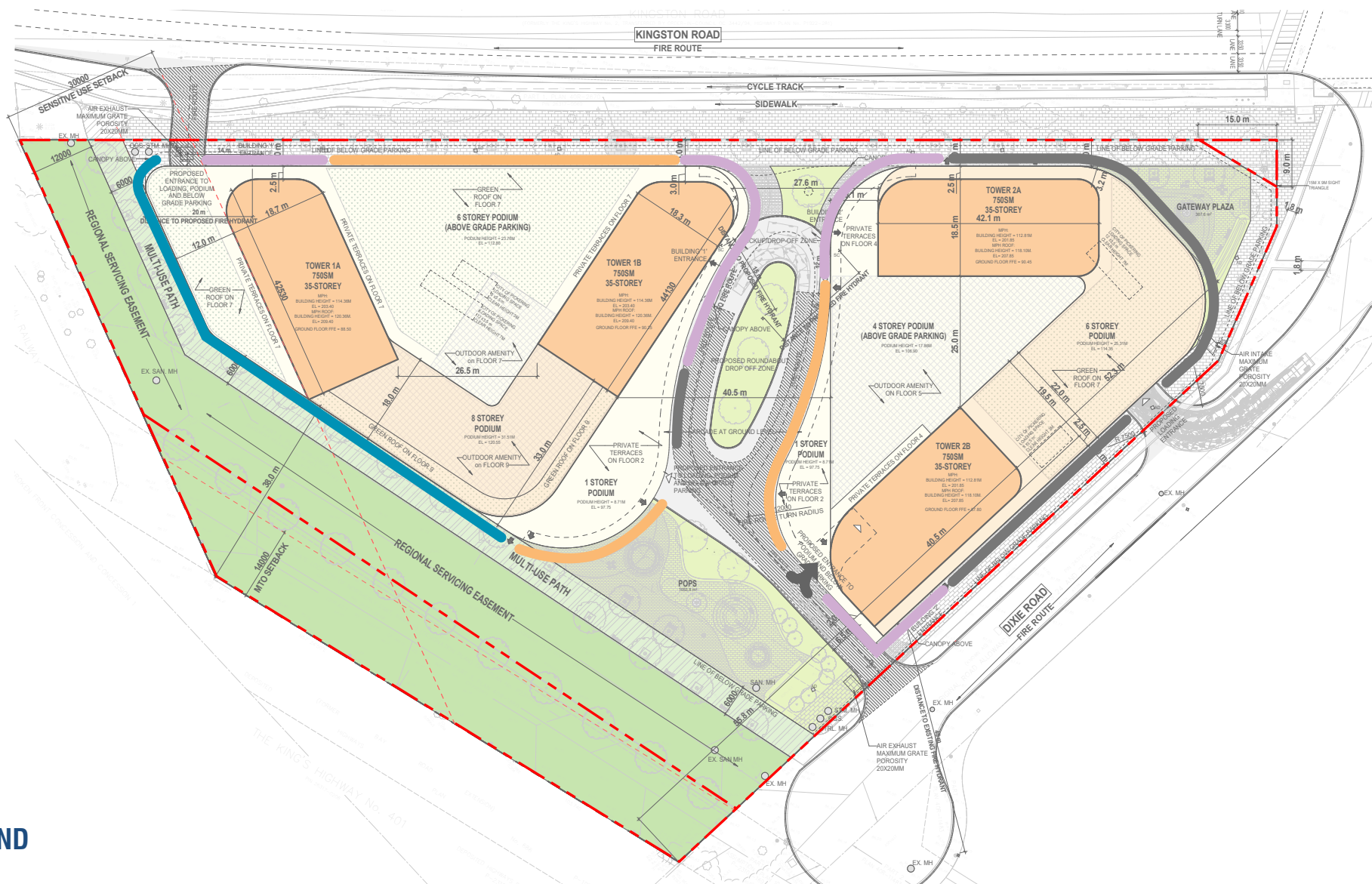


Figure 39. North Elevation (BDP. QUADRANGLE ARCHITECTS LTD.)

Figure 40. East Elevation (BDP. QUADRANGLE ARCHITECTS LTD.)





## LEGEND

- |                |                  |                            |                            |
|----------------|------------------|----------------------------|----------------------------|
| Subject Site   | Tower Element    | Upper Podium (5-8 Storeys) | Lower Podium (1-6 Storeys) |
| Lobby Frontage | Amenity Frontage | Indoor Parking Frontage    | Loading/ Frontage          |

Figure 47. Site Organization





## 3.2. Urban Design Vision

### URBAN DESIGN VISION

The urban design vision for the proposed development is to create pedestrian-friendly streetscapes through innovative and sustainable building and landscape designs. Contemporary building solutions will combine with contemporary architectural styles to maintain the character and image of the community while contributing to a denser more sustainable future.

### SITE ORGANIZATION

The proposed building is situated on the south side of Kingston Road and west of Dixie Road. The proposal is comprised of two buildings with two towers each. Building podiums of four to eight-storeys front along Kingston Road and Dixie Road. A new centralized drop-off loop is proposed between the two buildings and is accessed via Dixie Road.

The building podiums are both roughly triangular in shape as a result of the shape of the Subject Site. The podiums are setback 5.0 metres from the property lines fronting along Kingston Road and Dixie Road. The towers and upper podiums form a V-shape, which create a naturally protected space for outdoor amenity space. All four towers are 35-storeys in height and have separation distances of 25.0 metres or more. The towers generally setback 2.5 metres from the podiums at levels seven.

Substantial indoor amenity space will be provided along the Kingston Road frontage to help create an active pedestrian realm.

Vehicle access to parking, servicing and loading is provided from two access points on Dixie Road. A third access right-in-right-out vehicle access is located on Kingston Road along the western edge of the Subject Site.

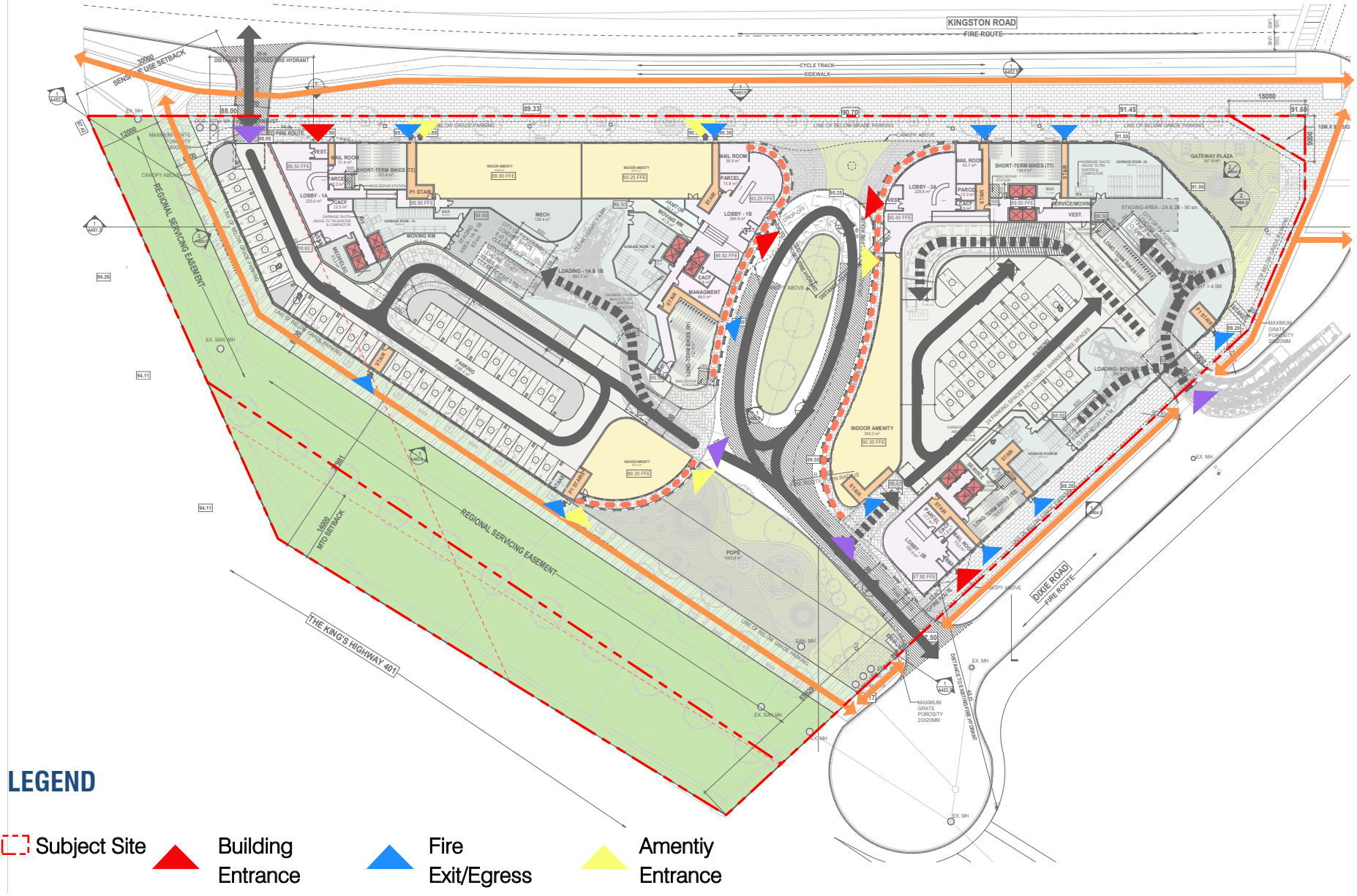


Figure 48. Pedestrian/Vehicular Circulation and Access



## **PEDESTRIAN/VEHICULAR CIRCULATION AND ACCESS**

Pedestrian circulation will occur on public sidewalks that surround the Subject Site along Kingston Road and Dixie Road. A cycle track along Kingston Road is also proposed as indicated in the Dunbarton/Liverpool Precinct Connectivity Plan (Figure 21).

A multi-use path is proposed along the southwest portion of the Subject Site adjacent to the 14.0 metre MTO setback. This pathway will link Kingston Road and Dixie Road and connect with future developments to the east.

Pedestrian access to the main entrances is via Kingston Road and Dixie Road. The entrance for building 1A is accessed from Kingston Road along the western edge of the site. Access to the residential lobby of 1B and 2A are located

towards the north end of the drop-off loop near Kingston Road. The Residential Lobby for building 2B is accessed via Dixie Road near the newly proposed driveway. A centrally located drop-off loop situated between buildings 1 and 2 is framed by a pedestrian arcade which will provide covered pedestrian circulation space.

Vehicular access to the site is provided from two access points on Dixie Road. A third access right-in-right-out vehicle access is located on Kingston Road. The southern most Dixie Road access point connects to the centrally located drop-off loop which is situated between the two buildings. This driveway also connects to the Service, Loading and Garbage bays for Building 1. Service, Loading and Garbage for Building 2 is accessed off of Dixie Road. This entrance location does not present any conflict for the neighbouring lands to the east.



-  Subject Site
  Regional Servicing Easement
  Multi-use Path
  POPS/Gateway Plaza
  Amenity Space
  Private Terrace

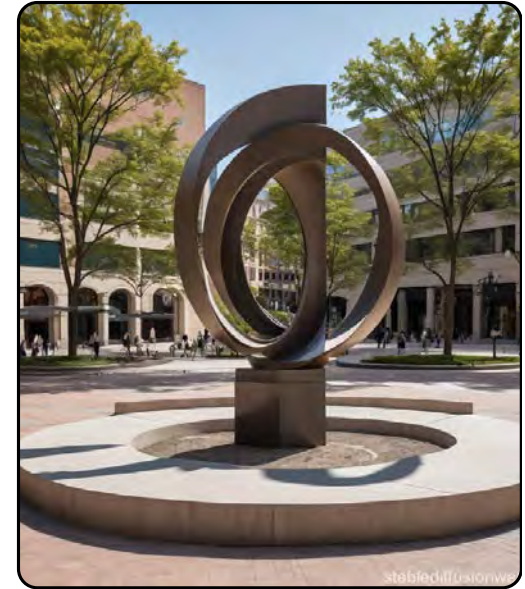
Figure 49. Amenity Area Ground Floor and Mezzanine Level



Figure 50. Landscape Plan (MHBC planning, urban design & landscape architecture)



blending geometry and  
playfulness to create  
enticing public spaces



variety of  
amenity facilities



integrated planting and  
seating opportunities



wayfinding  
urban  
elements

Figure 51. Landscape Inspiration Precedents (MHBC planning, urban design & landscape architecture)





Figure 52. View of Gateway Plaza (BDP. QUADRANGLE ARCHITECTS LTD.)

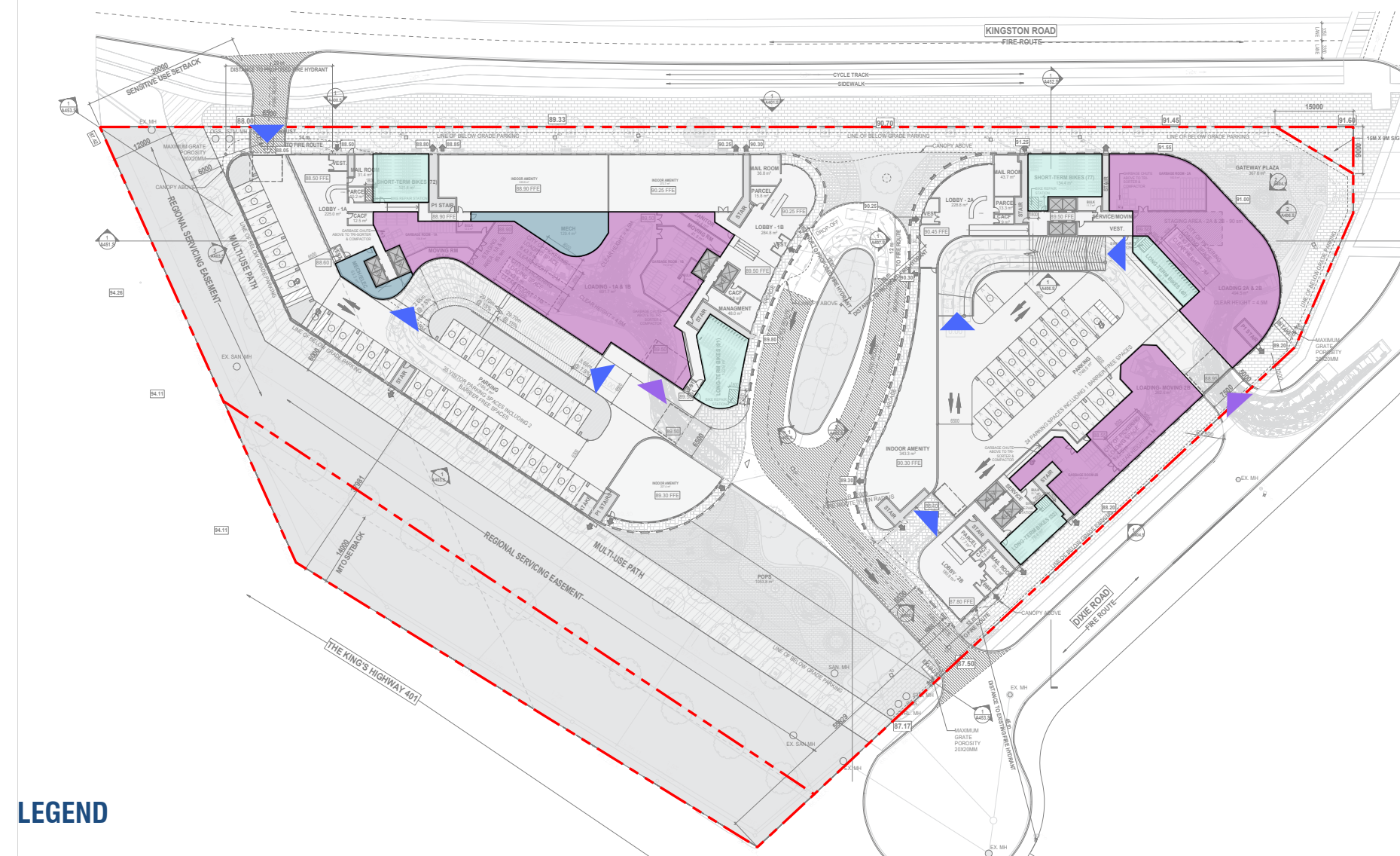
## LANDSCAPE AND AMENITY AREAS

Landscaping planting beds and unit paving is proposed along the building edges along Kingston Road and Dixie Road to enhance the aesthetics and public realm. As previously mentioned, along Kingston Road a new cycle track is proposed as well as sidewalks along Kingston Road and Dixie Road. These elements will all improve the public realm and encourage active modes of transportation.

As previously mentioned, beside the 14.0 metre MTO setback is a proposed 6.0 metre wide multi-use path which will link Kingston Road and Dixie Road. This multi-use path will also connect to paths or walkways on the adjacent site across Dixie Road. A POPS (privately owned public space) is proposed south of the new driveway connected to Dixie Road and adjacent to the multi-use path. At the corner of Kingston Road and Dixie Road, a Gateway Plaza is proposed.

Along Kingston Road, indoor amenity space is proposed at the base of Building 1 and the mezzanine level of Building 2. These active uses at grade will create a positive pedestrian realm along Kingston Road and will provide eyes on the street. More amenity space on level 1 can be found in the southern corner of Building 1, adjacent to the POPS and fronting along the drop-off loop on the western side of Building 2. Additional indoor amenity space is provided on the Mezzanine and levels 5 of Building 2 and levels 7 and 9 of Building 1. Similarly outdoor amenity space will be provided on Levels 7 and 9. 3,000 square metres of indoor and 2,985 square metres of outdoor amenity space is proposed for a total of 5,985 square metres.

Private outdoor terraces are proposed for units on Level 2, 5 and 7.



LEGEND

- Subject Site
- Internalized Waste Storage and Loading
- Underground Parking Access/Ramp
- Service Access
- Bicycle Storage/ Bicycle Elevator
- Other B.O.H. (Elec. Room, Mech. Room, etc.)

Figure 53. Parking, Loading and Servicing -Level 1



## PARKING, LOADING AND SERVICING

“Back-of-house” activities including loading, servicing and waste collection are located within the building podiums on level 1. These services are kept out of the way and often tucked behind active uses such as lobbies and indoor amenity. In situations where these back-of-house uses are flush up against an exterior wall, great care will be taken to ensure the facade is appropriately designed. Blank, concrete walls will not be incorporated in these situations.

As previously indicated vehicular access to these BOH uses are from the new driveway from Dixie Road. The location of this access point does not present any conflict for the neighbouring lands to the east.

A total of 746 long-term and 149 short-term bicycle parking spaces are provided. There will also be provision for 111 long-term electric bike parking spaces.

Parking for the Proposed Development is provided via one below-grade parking level and seven levels of podium parking. Podium parking is often ‘wrapped’ in residential or other active uses. In situations where the podium parking is present at the exterior face of the building, special design and treatment will be paid to the facade to ensure large blank surface walls are not present. Facade materials, fenestration and articulation will ensure these facades are interesting and engaging.

Figure 54. Buildings Elevation (BDP. QUADRANGLE ARCHITECTS LTD.)





Figure 55. View from Kingston Road Rendering (BDP. QUADRANGLE ARCHITECTS LTD.)



## BUILT FORM

### HEIGHT, MASSING AND SETBACKS

The proposal consists of tower heights of 35-storey and podiums of 6-storeys to 8-storeys (including the mezzanine level). The proposed development includes setbacks of 5.0 metres from the north and east property line and 38.0 metres from south property line (including the 14.0 metre MTO setback). An arcade is provided adjacent to the drop-off loop on the ground floor which will create a natural overhang and protection from the elements.

Towers stepback 2.5 and 3.0 metres from the podium. The separation distance between the two towers is 27.5 metres ensuring access to natural light, views to the sky and privacy between adjacent units. This will also preserve views to the sky from the street level, reduce shadowing and create an elegant profile in the skyline.

### BUILDING PODIUM, TOWER AND TOP

Urban Design best practices encourage towers to be comprised of three components; podium, tower and tower top. The proposed 6- to 8-storey building base (podium) will create an appropriate streetwall along Kingston Road.

The building podium is appropriately setback from the property line. Towers stepback from the podium, while each tower has a distinct mechanical penthouse.

# CONCLUSION

The proposed development conforms with urban design best practices as well as the relevant criteria within Provincial, Regional and Municipal policies. Urban design and architectural strategies will be incorporated in order to promote a safe, pedestrian-friendly and comfortable built environment. Additional attention and enhancements will be made to the ground floor to activate the street level and shield podium parking from the public realm. The established community character will be expanded through complementary building designs and landscape design strategies.

A

**APPENDIX**



## A. Shadow Study

### SHADOW STUDY ANALYSIS

The following Sun/Shadow Study was prepared by BDP. Quadrangle Architects Ltd. to illustrate the shadow impacts of the proposed residential development at 1095 Kingston Road in Pickering. The proposed development consists of 4 towers of 35-storeys with podiums ranging between 6-storeys to 8-storeys. The surrounding context of the Subject Site primarily constitutes low-rise commercial plazas that front onto Kingston Road residential uses including townhouses, detached, and semi-detached units, and the major highway corridor located to the south.

The study has been prepared using the following criteria:

- Test Times of March 21st, June 21st, September 21st and December 21st at the hours of 9:18am, 10:18am, 11:18am, 12:18pm, 1:18pm, 2:28pm, 3:18pm, 4:18pm, 5:18pm, 6:18pm and 7:18pm;
- Surrounding streets, public facilities, and existing buildings have been included in the model; and
- A metric bar scale and north arrow has been included on each sheet along.

**MARCH 21ST SHADOW STUDY**

At 9:18 am on March 21st, Kingston Road, the railway tracks and some single-family homes northeast of the Subject Site experience shadowing from the proposed development. Shadowing continues on single family homes on the north side of Kingston Road until 2:18. Given the slender nature of the towers, these shadows move quickly.

Throughout March 21st, no shadows from the proposed development are cast onto the outdoor daycare area at the Dunbarton-Fairport United Church.

At 4:18 pm and onward shadows from the proposed development impact the existing commercial businesses and associated parking lots.

**JUNE 21ST SHADOW STUDY**

From 9:18 am to 3:18 pm on June 21st, shadowing occurs along Kingston Road with no shadows reaching the single-family homes.

At 4:18 pm and onward shadows from the proposed development impact the existing commercial businesses and associated parking lots.

**SEPTEMBER 21ST SHADOW STUDY**

At 9:18 am on September 21st, Kingston Road, the railway tracks and some single-family homes northeast of the Subject Site experience shadowing from the proposed development. Shadowing continues on single family homes on the north side of Kingston Road until 2:18. Given the slender nature of the towers, these shadows move quickly.

The outdoor daycare area is not impacted by shadows cast by the proposed development throughout September 21st.

At 4:18 pm and onward shadows from the proposed development impact the existing commercial businesses and associated parking lots.

**DECEMBER 21ST SHADOW STUDY**

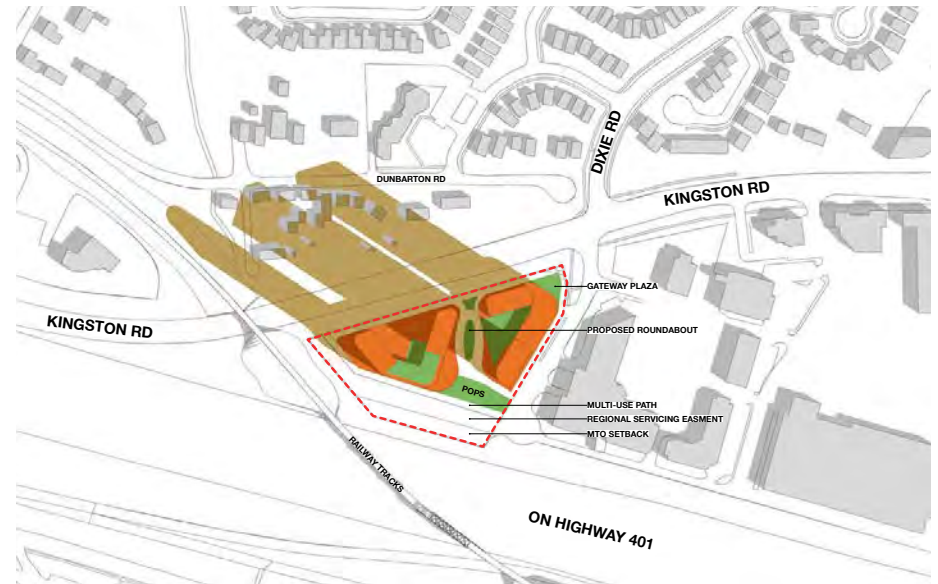
During winter months, the sun is lower and thus casts longer shadows than in the time previously analysed in this document. Shadows from the proposed development on December 21st stretch into the residential neighbourhood north of the Subject Site.

From 11:18 am to 2:18 pm on December 21st shadows are cast onto the outdoor daycare area at the Dunbarton-Fairport United Church.

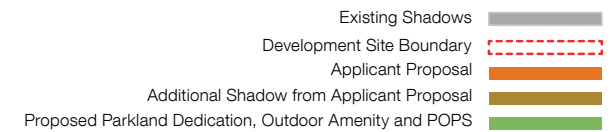




March 21 | 9:18 am



March 21 | 10:18 am



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

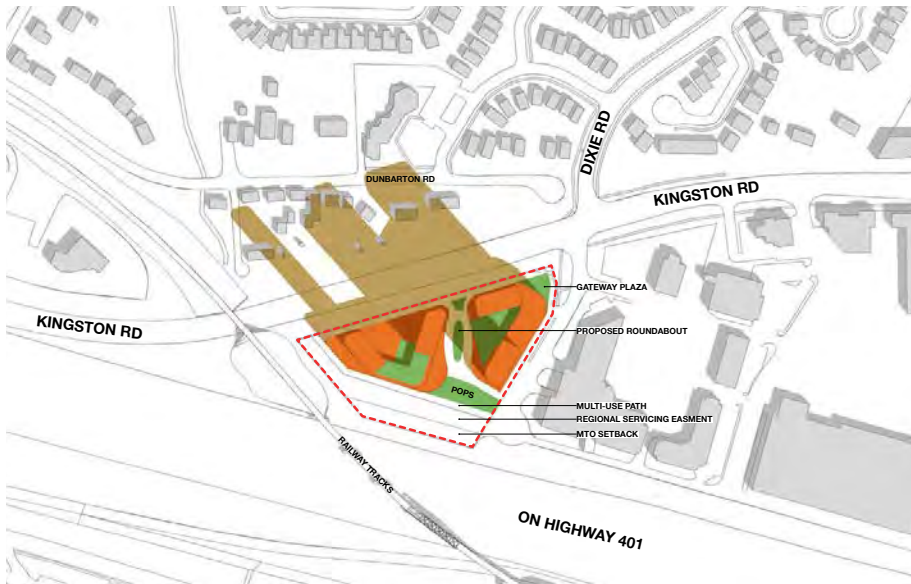
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 56. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

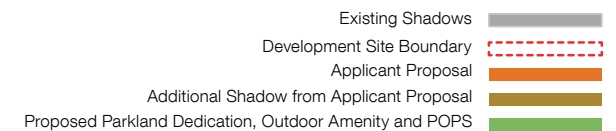




March 21 | 11:18 am



March 21 | 12:18 pm



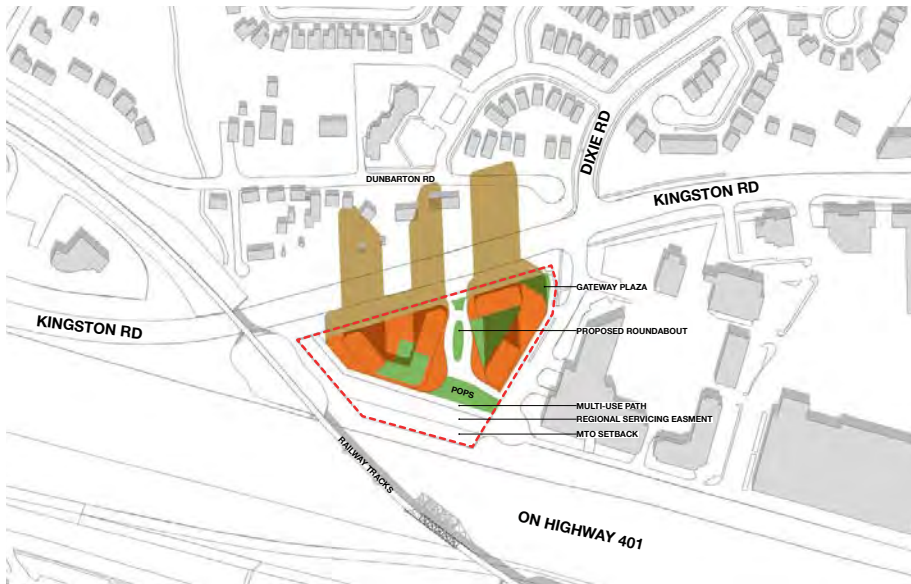
Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



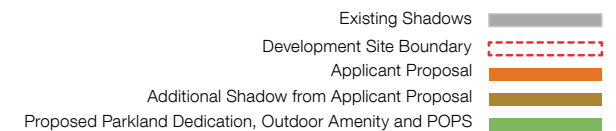
Figure 57. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



March 21 | 1:18 pm



March 21 | 2:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

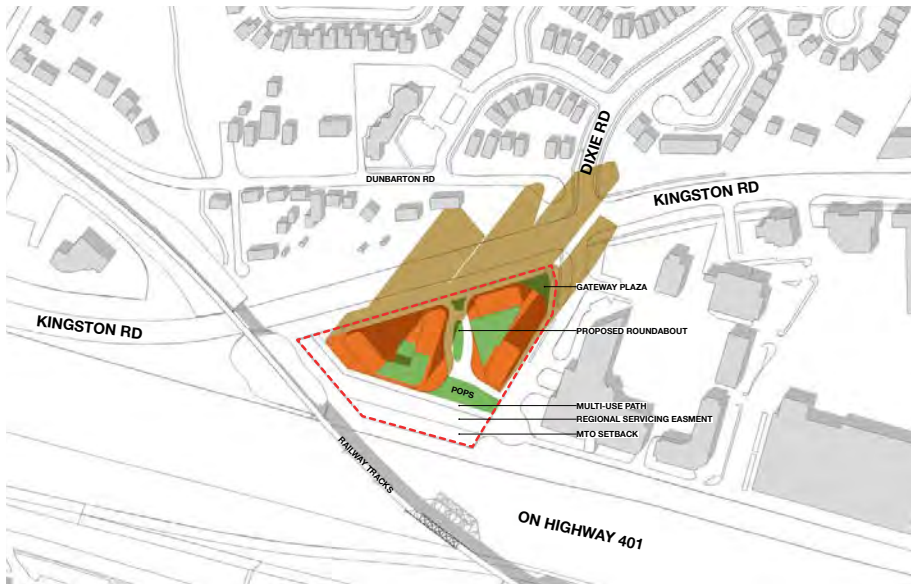
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 58. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

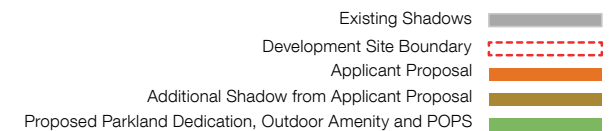




March 21 | 3:18 pm



March 21 | 4:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

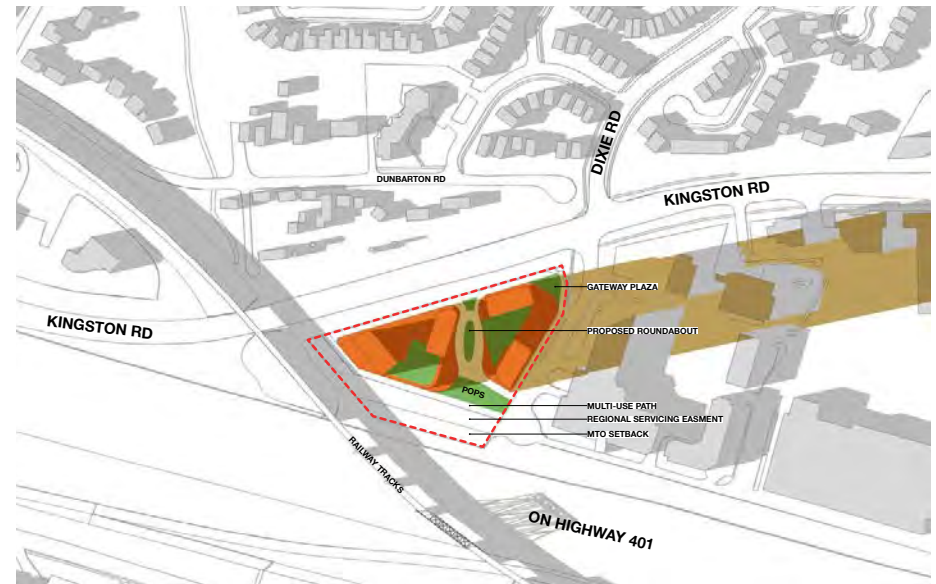


Figure 59. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

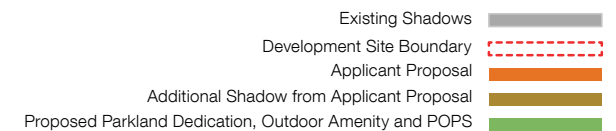




March 21 | 5:18 pm



March 21 | 6:18 pm



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**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



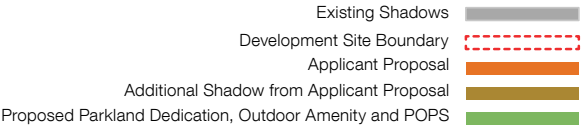
Figure 60. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)







March 21 | 7:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals



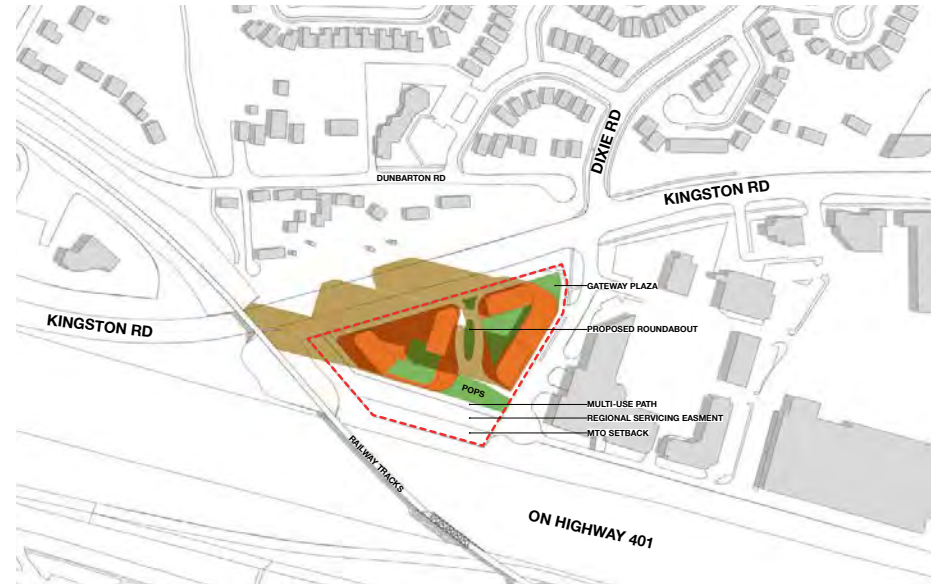
Project No. 21068 / 10 March 2025 / 1095 Kingston Road



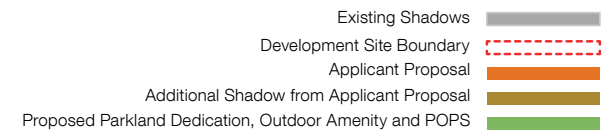
Figure 61. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



June 21 | 9:18 am



June 21 | 10:18 am



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

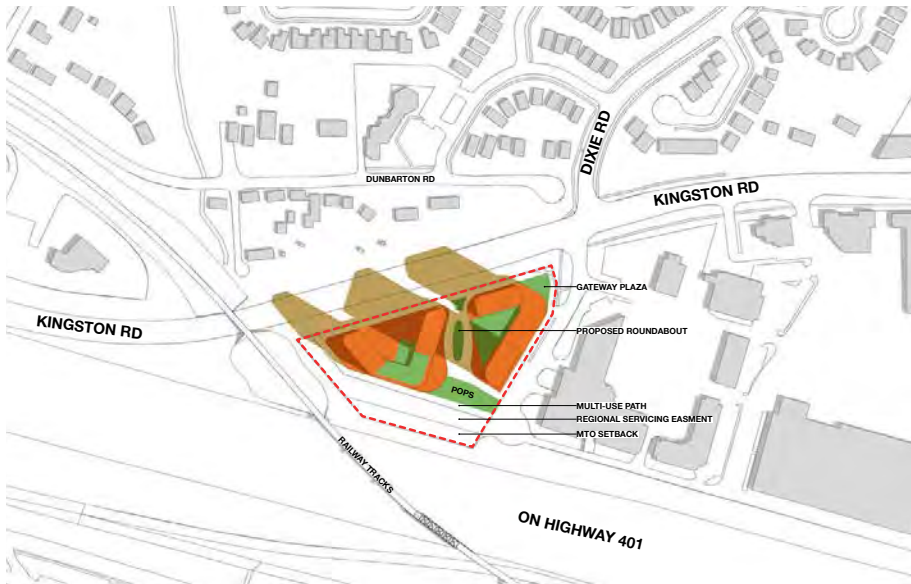
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 62. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

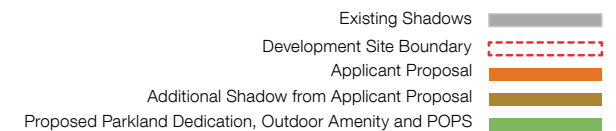




June 21 | 11:18 am



June 21 | 12:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



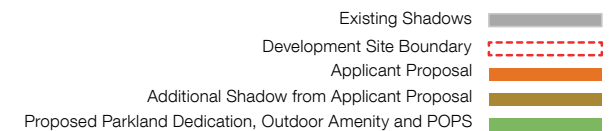
Figure 63. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



June 21 | 1:18 pm



June 21 | 2:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



10

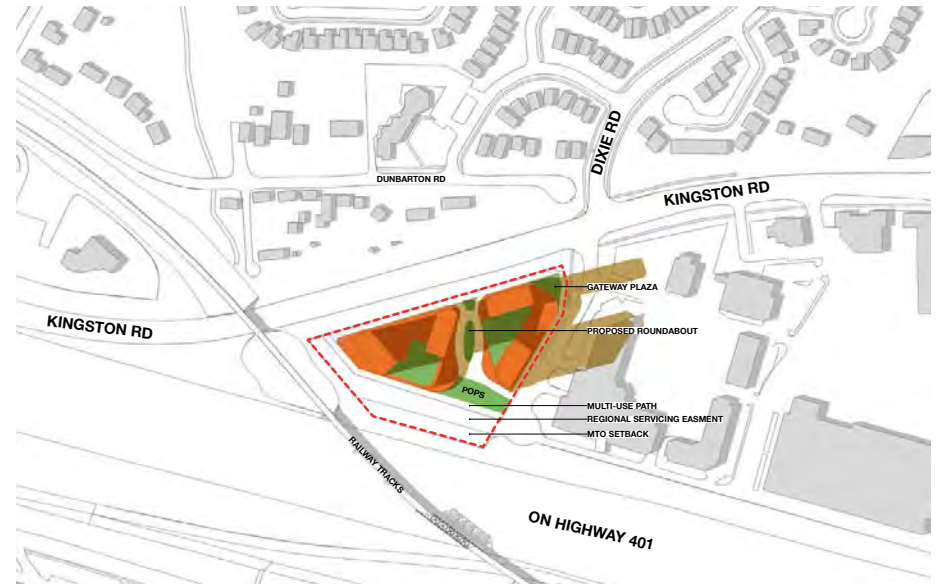
Figure 64. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



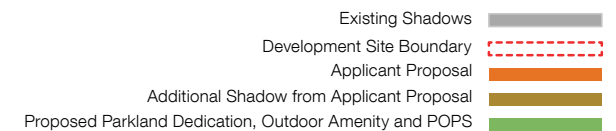




June 21 | 3:18 pm



June 21 | 4:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

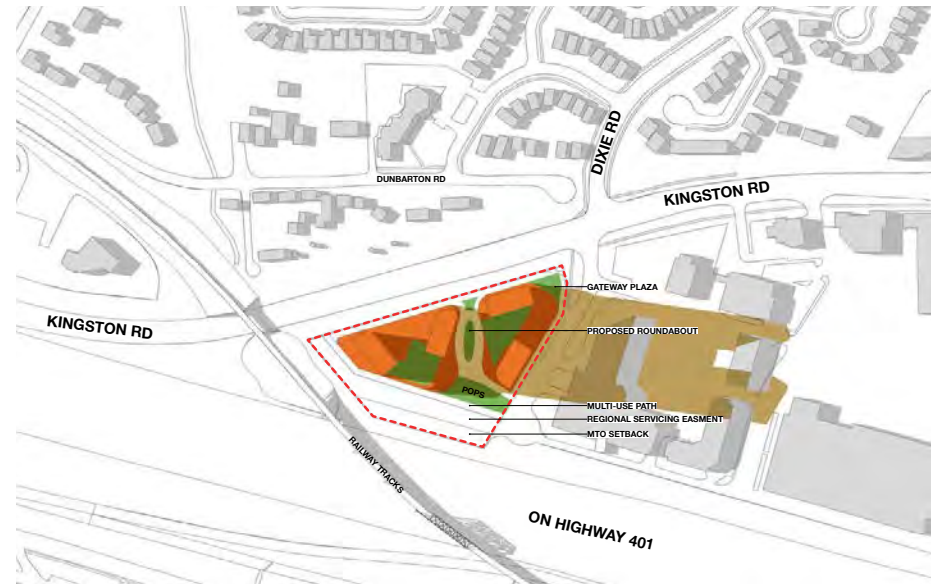


Figure 65. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

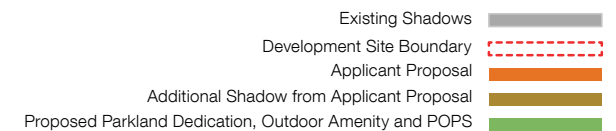




June 21 | 5:18 pm



June 21 | 6:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

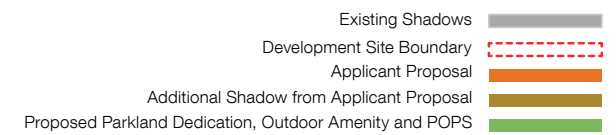


Figure 66. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)





June 21 | 7:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

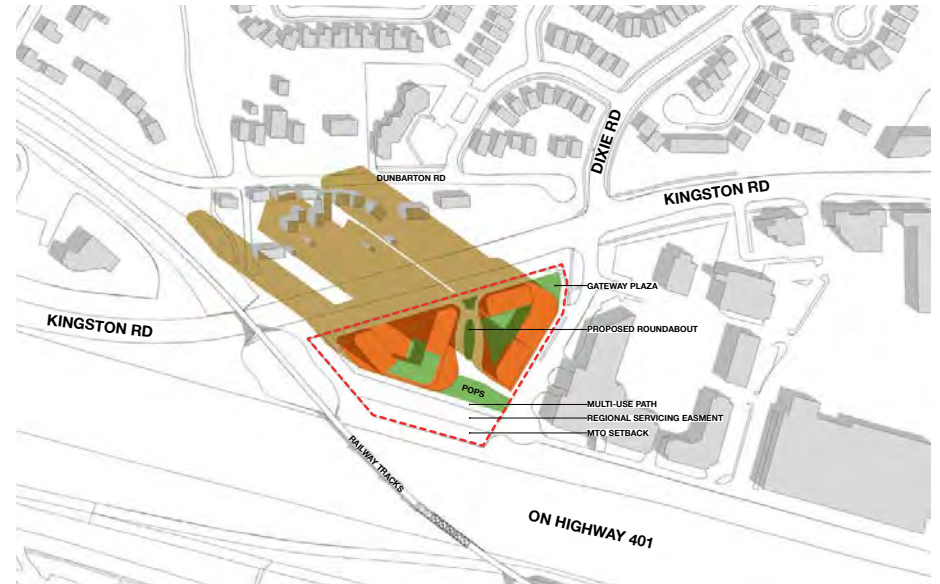
Project No. 21068 / 10 March 2025 / 1095 Kingston Road



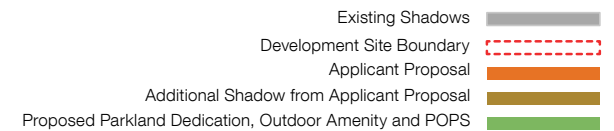
Figure 67. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



September 21 | 9:18 am



September 21 | 10:18 am



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

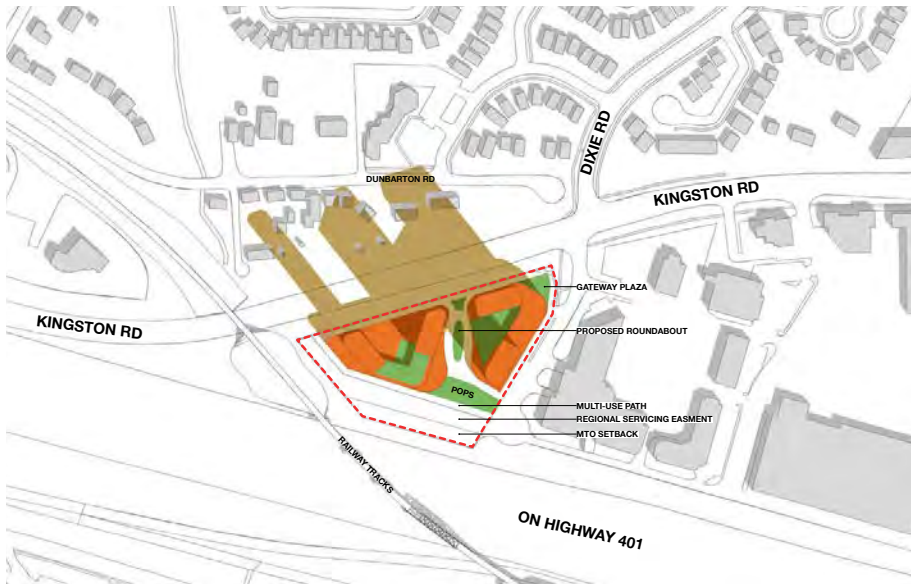
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

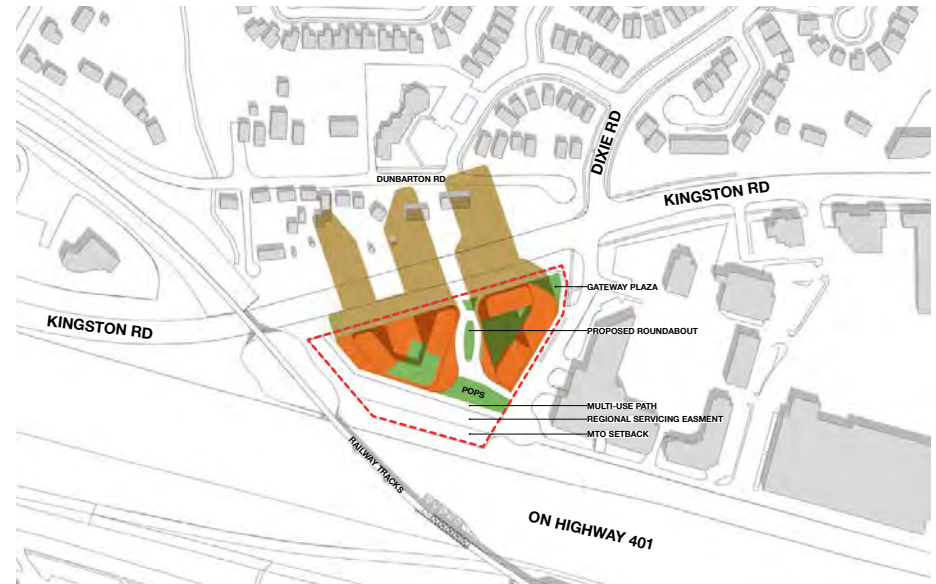


Figure 68. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

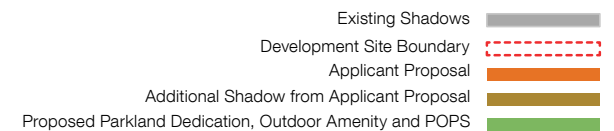




September 21 | 11:18 am



September 21 | 12:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

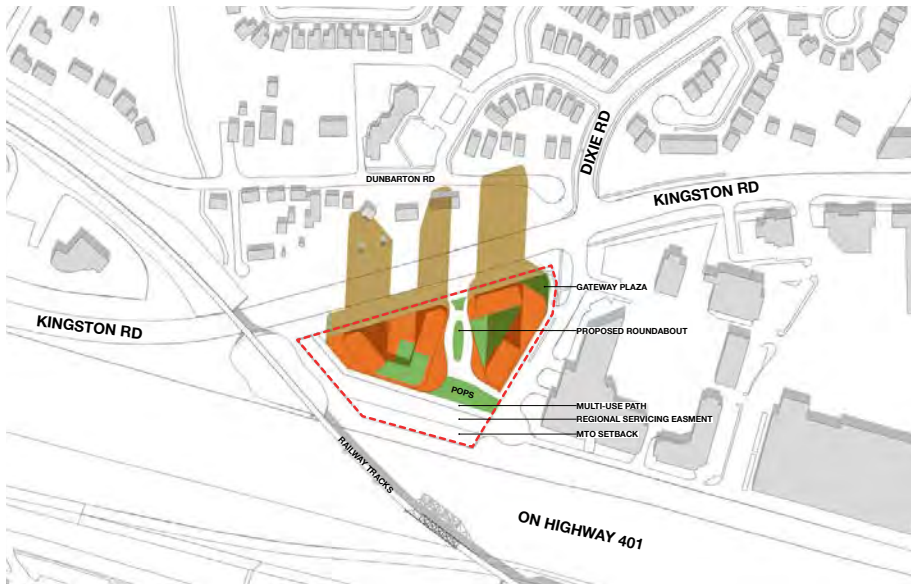
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 69. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

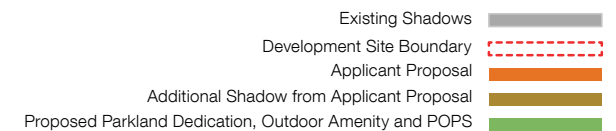




September 21 | 1:18 pm



September 21 | 2:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

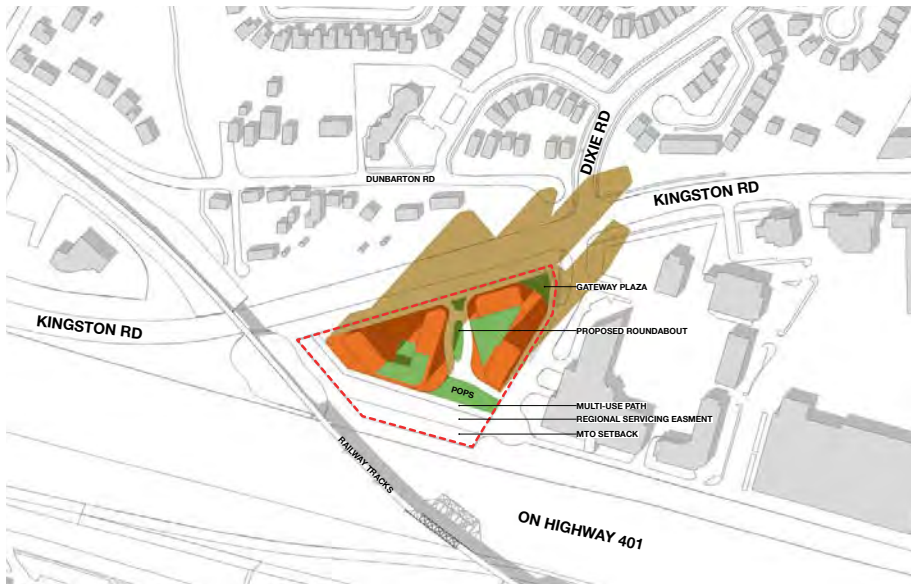
Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 70. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



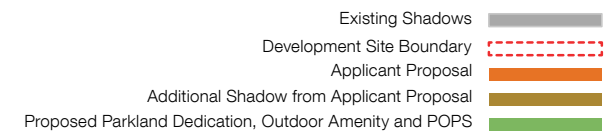




September 21 | 3:18 pm



September 21 | 4:18 pm



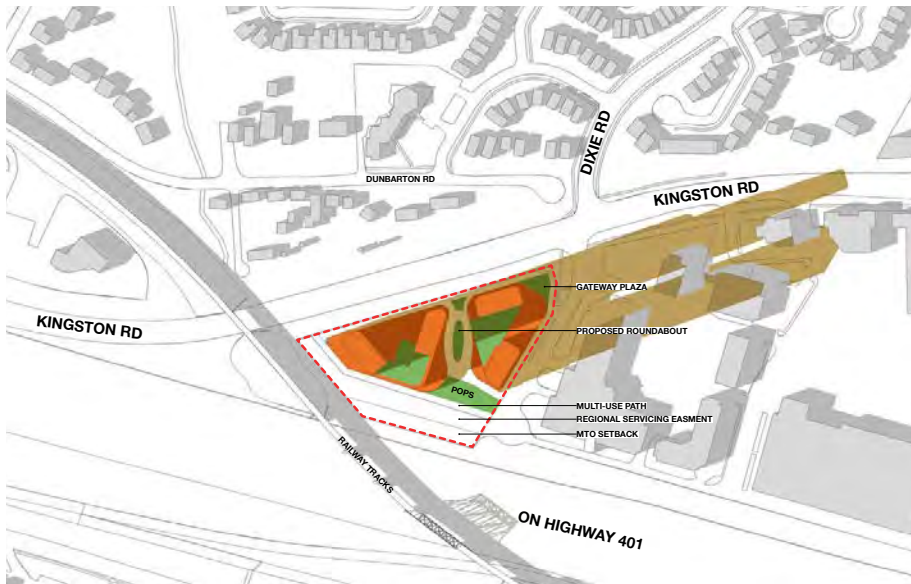
Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



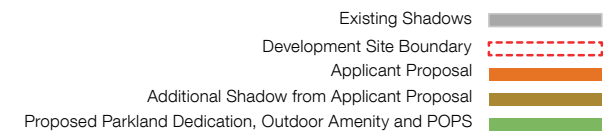
Figure 71. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



September 21 | 5:18 pm



September 21 | 6:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

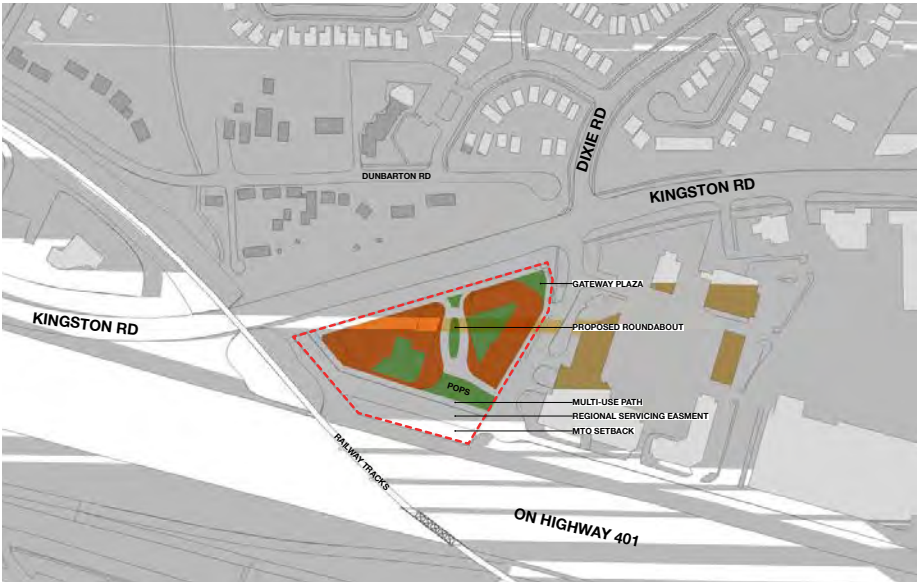
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

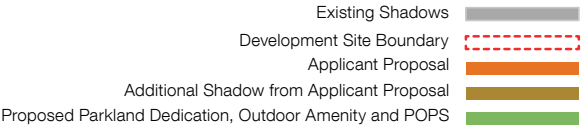


Figure 72. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)





September 21 | 7:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals



Project No. 21068 / 10 March 2025 / 1095 Kingston Road



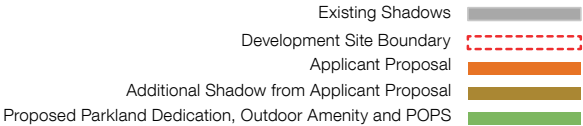
Figure 73. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



December 21 | 9:18 am



December 21 | 10:18 am



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



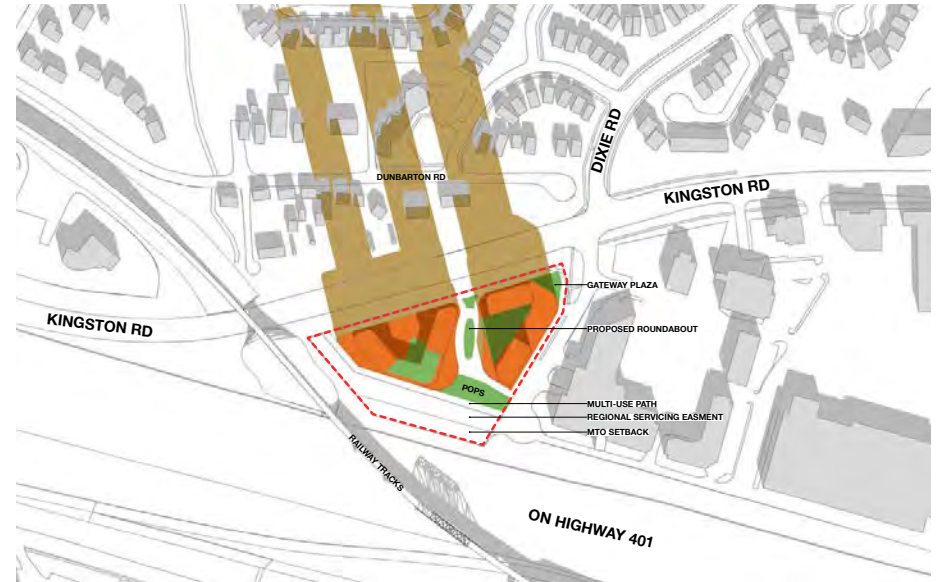
Figure 74. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



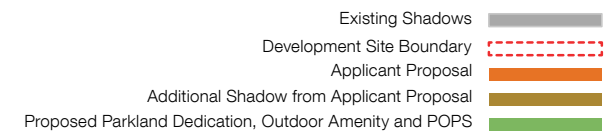




December 21 | 11:18 am



December 21 | 12:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

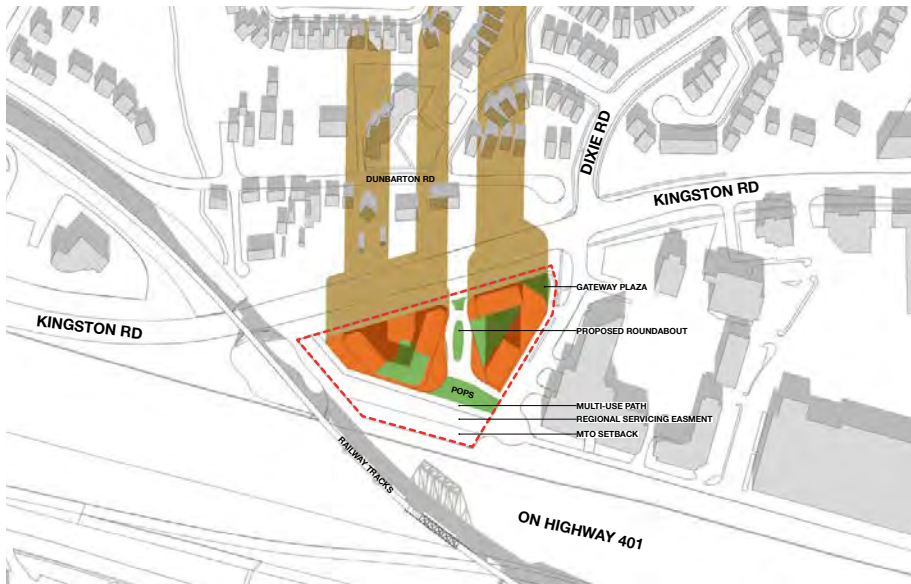
**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

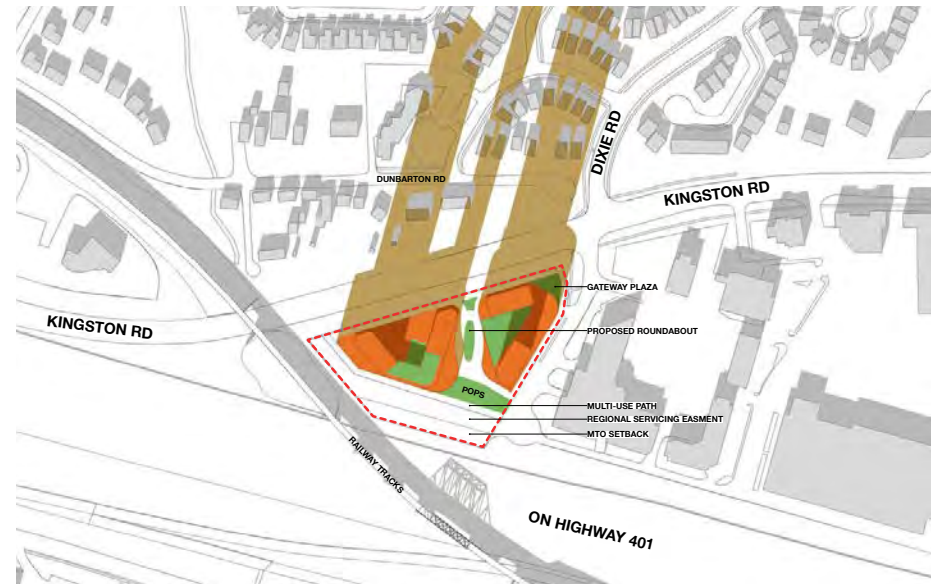


Figure 75. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

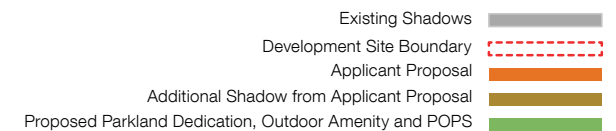




December 21 | 1:18 pm



December 21 | 2:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 76. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)

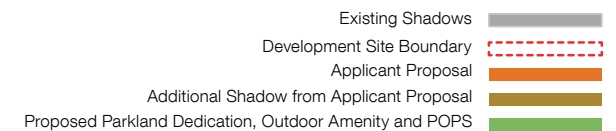




December 21 | 3:18 pm



December 21 | 4:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road



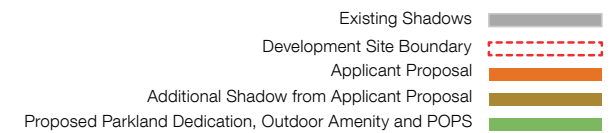
Figure 77. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



December 21 | 5:18 pm



December 21 | 6:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals

**BDP.**  
**Quadrangle**

Project No. 21068 / 10 March 2025 / 1095 Kingston Road

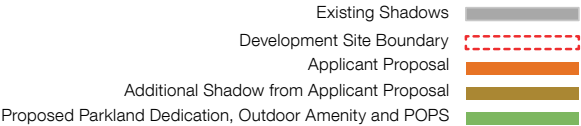


Figure 78. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)





December 21 | 7:18 pm



Plans, dimensions, and calculations are preliminary and approximate. Conceptual design as shown is subject to municipal approvals



Project No. 21068 / 10 March 2025 / 1095 Kingston Road



Figure 79. Shadow Study (BDP. QUADRANGLE ARCHITECTS LTD.)



## SHADOW STUDY SUMMARY

The shadow impacts of the proposed development on the neighbouring properties are acceptable in extent and duration. The shadows are acceptable for the following reasons:

- The majority of shadowing are cast on commercial parking lots;
- Shadowing on the existing single-family homes further north of the subject site are minor;
- Shadowing impact on the outdoor daycare area at the Dunbarton-Fairport United Church occur in colder months;

Shadowing impacts on the proposed development itself are considered acceptable in location, extent and duration for the following reasons:

- The Gateway Plaza experiences a high degree of shadow impacts throughout the year, during the late morning and early afternoon. The Gateway Plaza receives the most sun in the mornings and evenings, especially in the summer months. This may coincide with higher use of the Plaza as users travel to and from the future BRT station and make use of the Plaza.
- The POPS and Multi-use Path space along the site's southern edge of the site have relatively limited shadow impacts year-round. The most impactful shadows on these spaces are observed in the early mornings and late afternoons/early evenings, this is especially significant in the winter months. The relatively high amount of sunlight here will benefit users leveraging these public spaces for the proposed amenities.
- The mid-block connection, inclusive of the pick-up/drop-off loop will be highly shadowed throughout the year. Due to its narrow north-south orientation, this area receives most light during the early afternoon before podium volumes cover it in shadows throughout the afternoon.

- The proposed podium amenity spaces on both Building 1 and Building 2 will receive ample sunlight in the mornings and early afternoons throughout the year. Shadow impacts will be most pronounced during the late afternoon and in the winter months. Due to the proposed tower placement and orientation, the podium amenities on Building 2 are expected to experience more shadows when compared to those of Building 1.

In conclusion, it is our opinion that the shadow impacts from the proposed development on the existing surrounding area are acceptable.